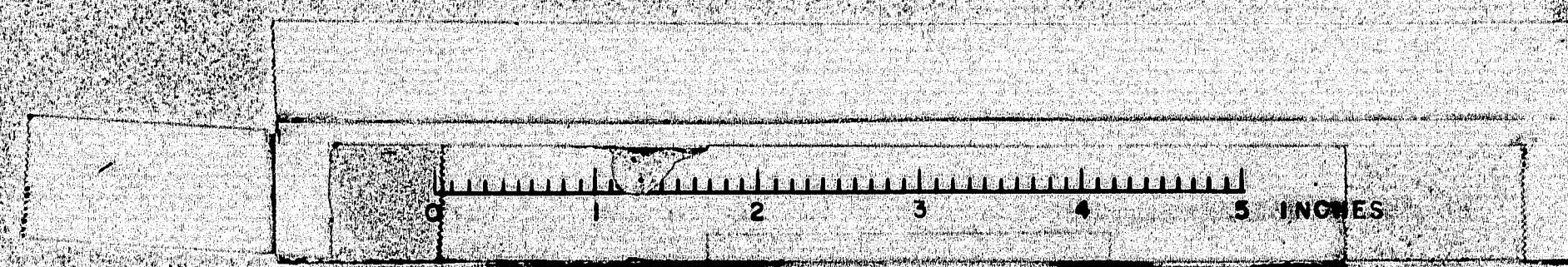
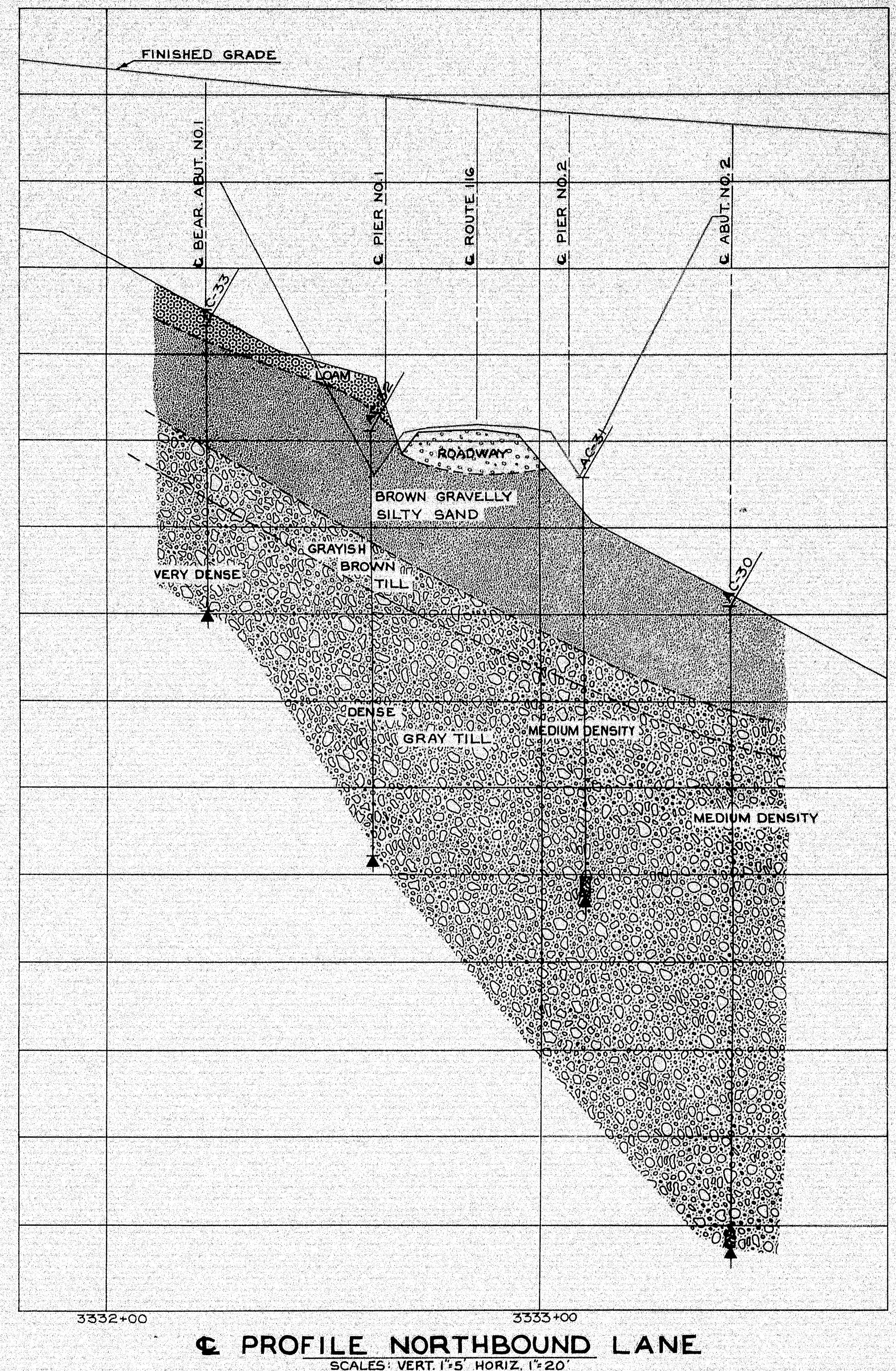
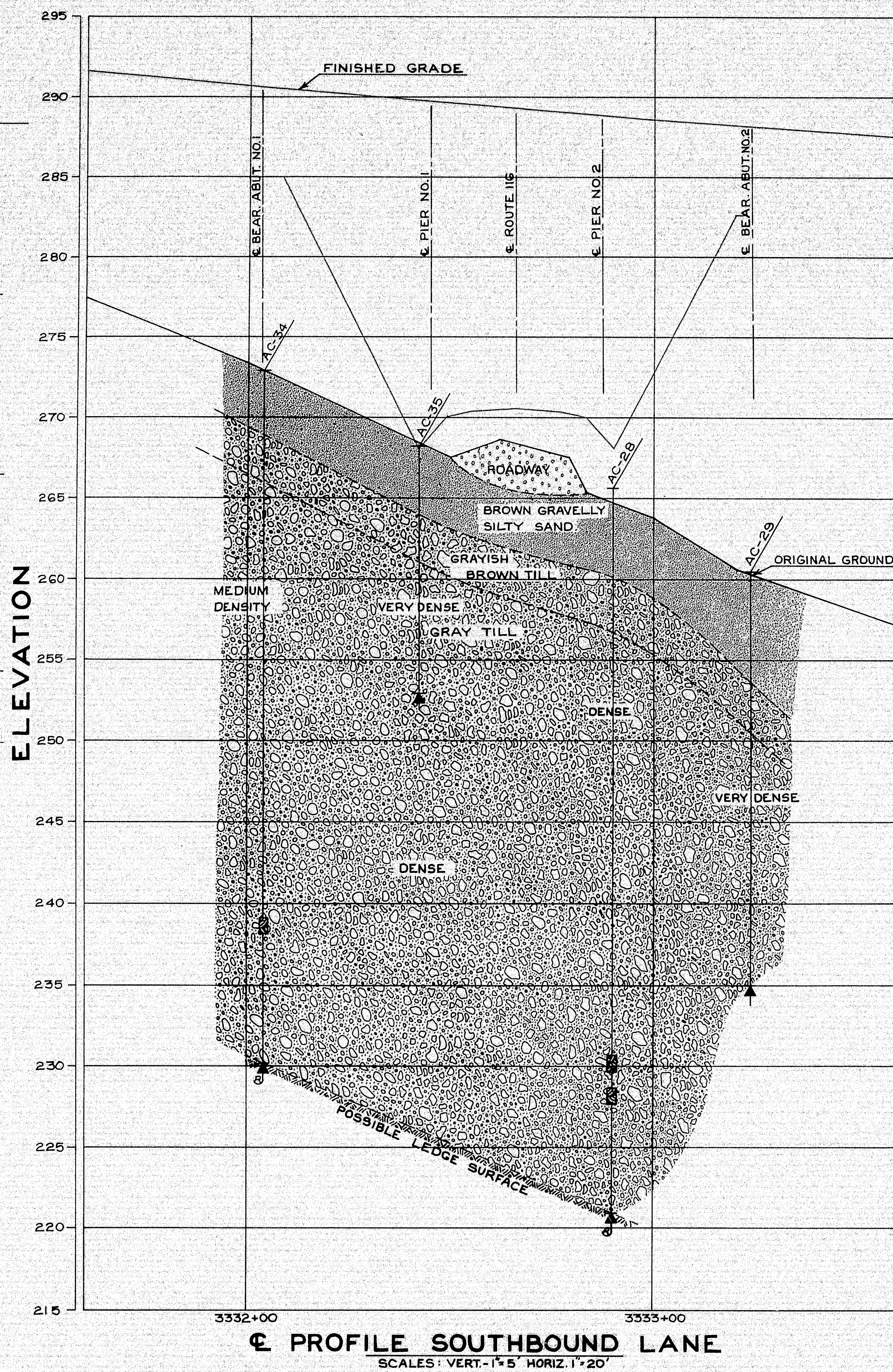
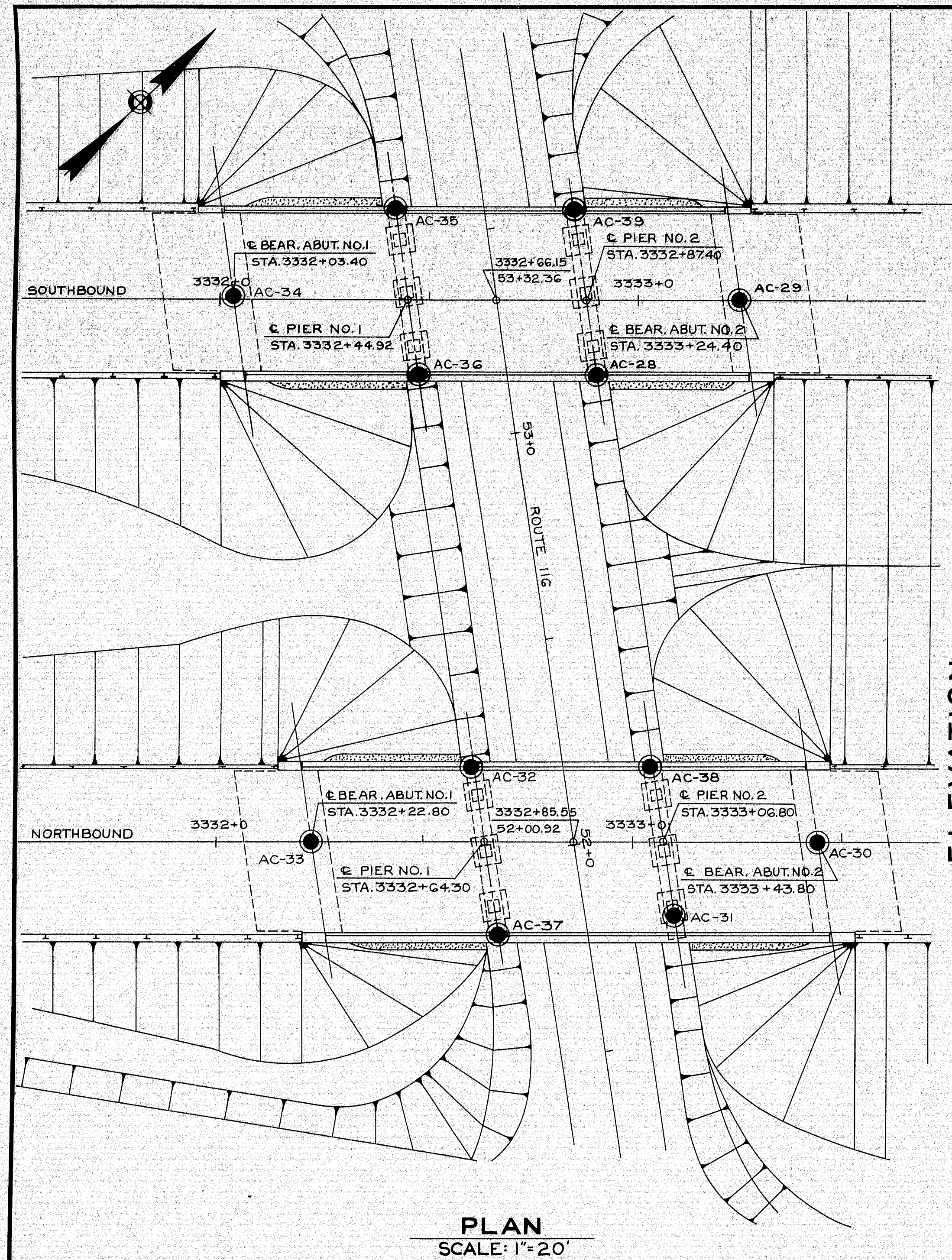


SPECIFICATIONS
 DESIGN: AASHTO Standard Specifications for Highway Bridges 1961, with Interim Specifications 1961/1964.
 CONTRACT: State of Maine State Highway Commission, Standard Specifications for Highways & Bridges, Revision of January 1956, and Supplemental Specifications, February 1960.
LIVE LOADING
 HS20-44 as modified for Interstate Highways.
ALLOWABLE STRESSES
 CONCRETE - $f_c = 1200$ p.s.i. $n = 10$
 REINFORCING STEEL - Intermediate Grades A, 20,000 p.s.i.
 STRUCTURAL STEEL - A36 - 20,000 p.s.i.
CONCRETE CLASSIFICATION
 ALL CONCRETE - Class "A"
STRUCTURAL STEEL CLASSIFICATION
 Except as otherwise noted, all standard details at material shall conform to A.S.T.M. designation A36.

DESIGN - CDH
 TRACE - JLA
 CHECK - HAO
 BRIDGE NO. SURVEY - PLOT
 STATE HIGHWAY COMMISSION
 BRIDGE DIVISION
INTERSTATE 95
 OVER
ROUTE 116
 IN THE TOWN OF
MEDWAY
PENOBSCOT COUNTY
 GENERAL PLAN & ELEVATION
 SHEET 51 OF 93 AUGUSTA, MAINE, OCT. 1964

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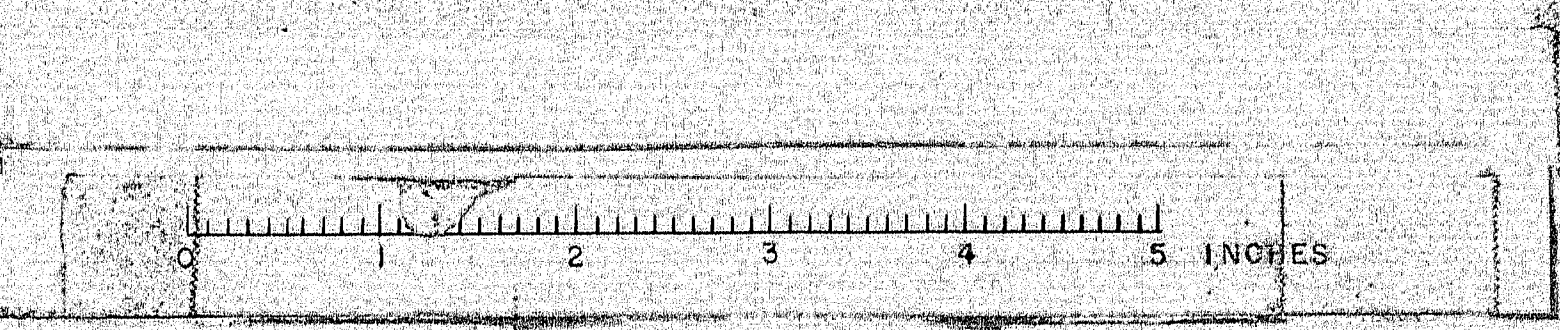


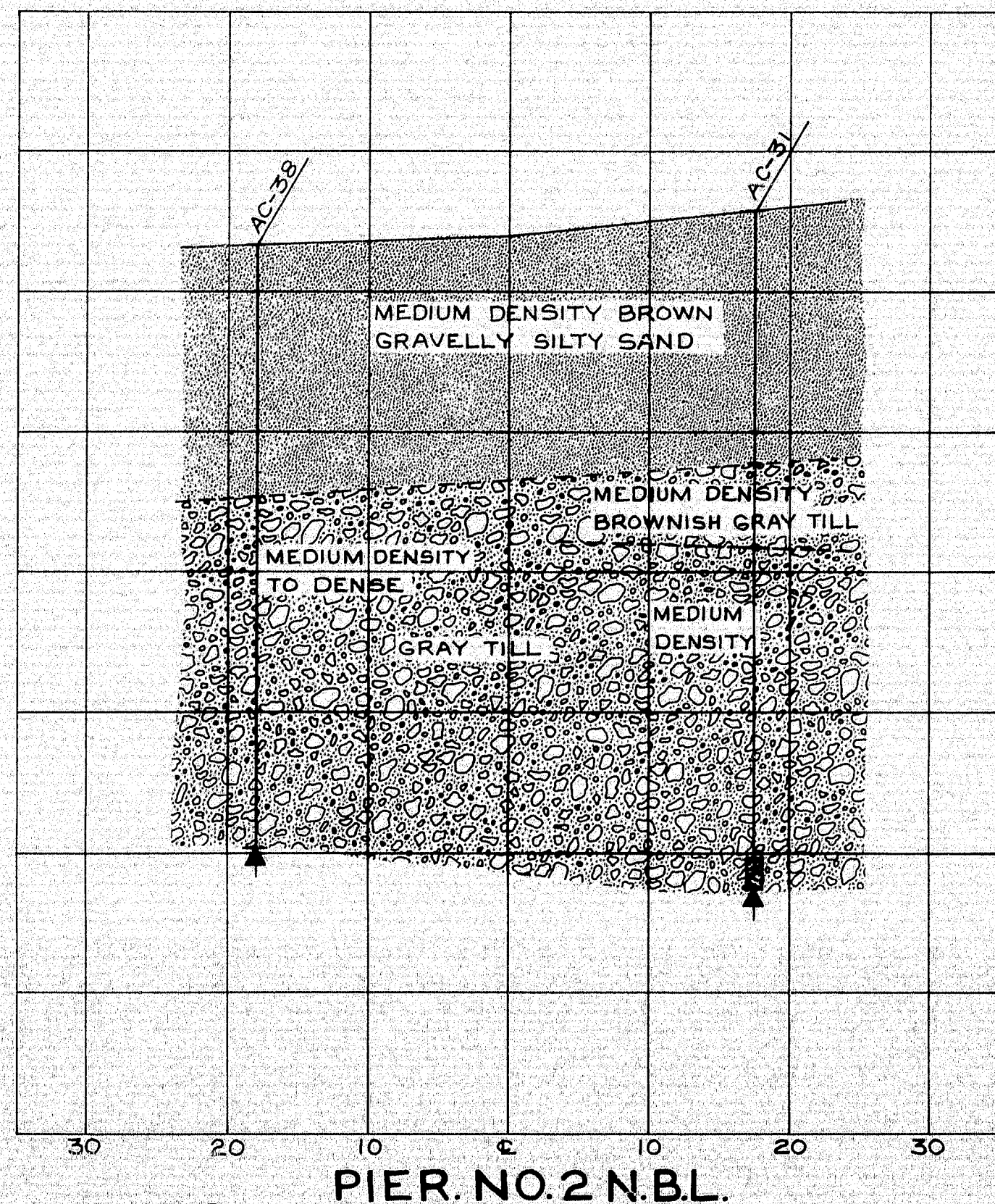
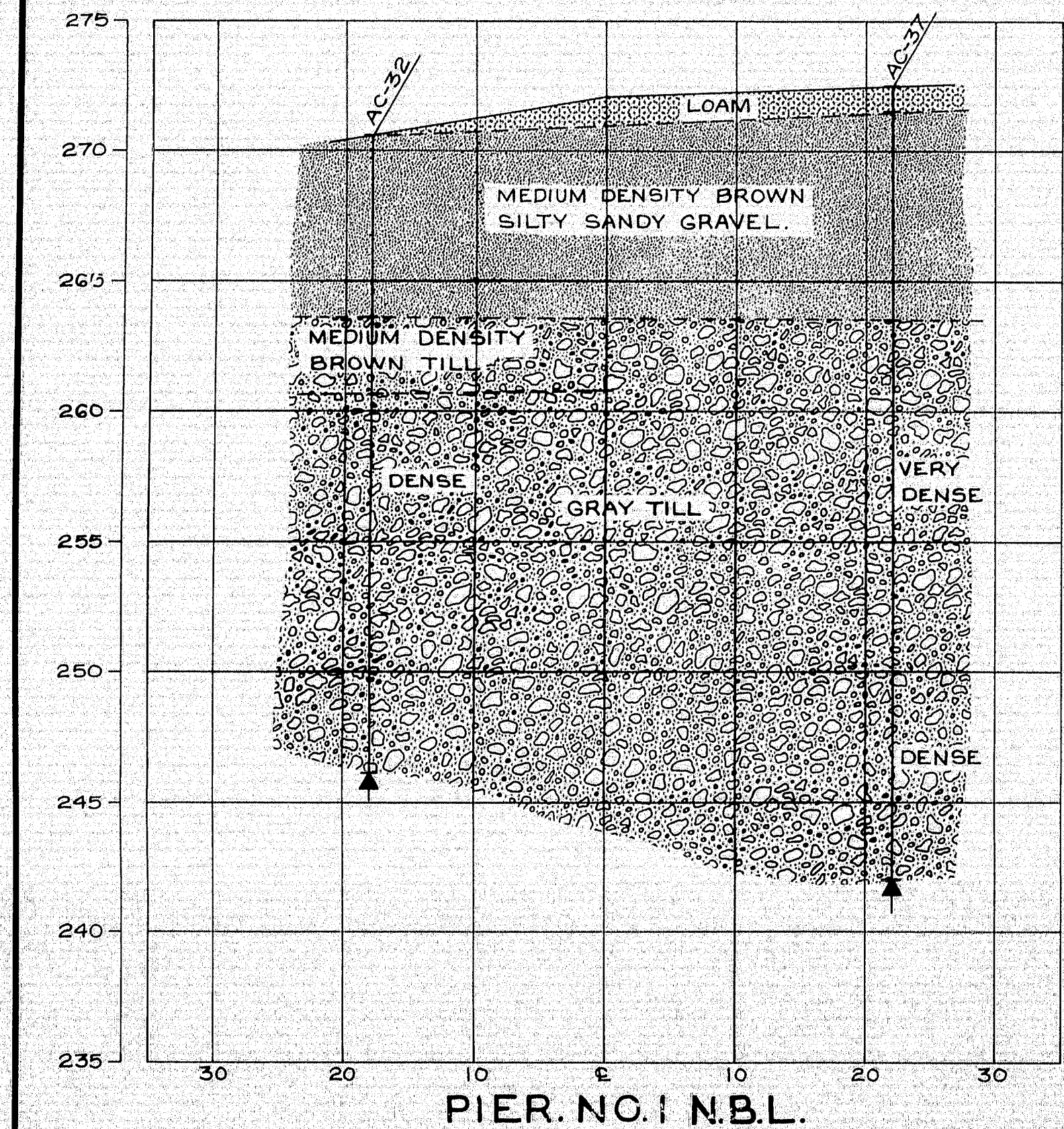
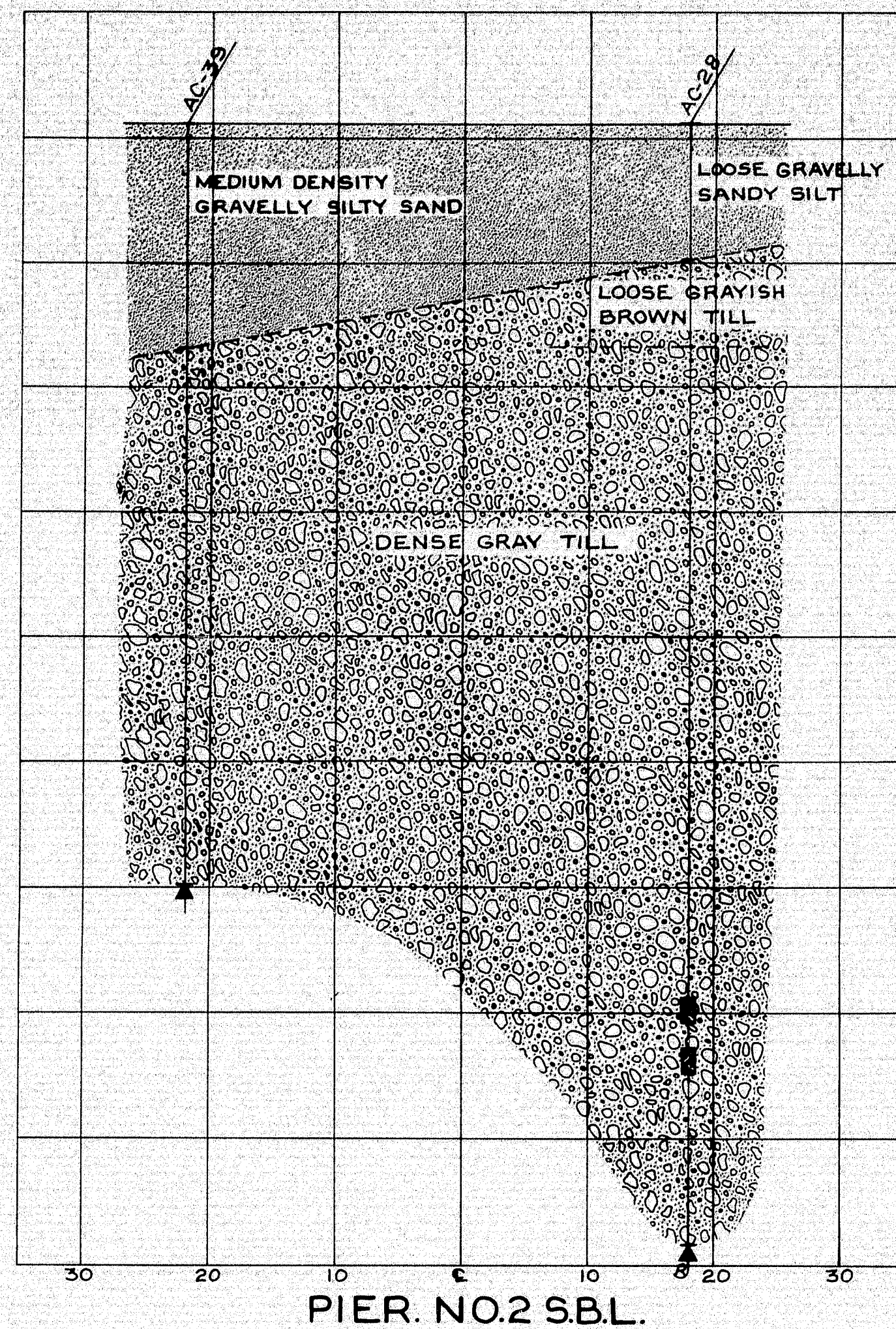
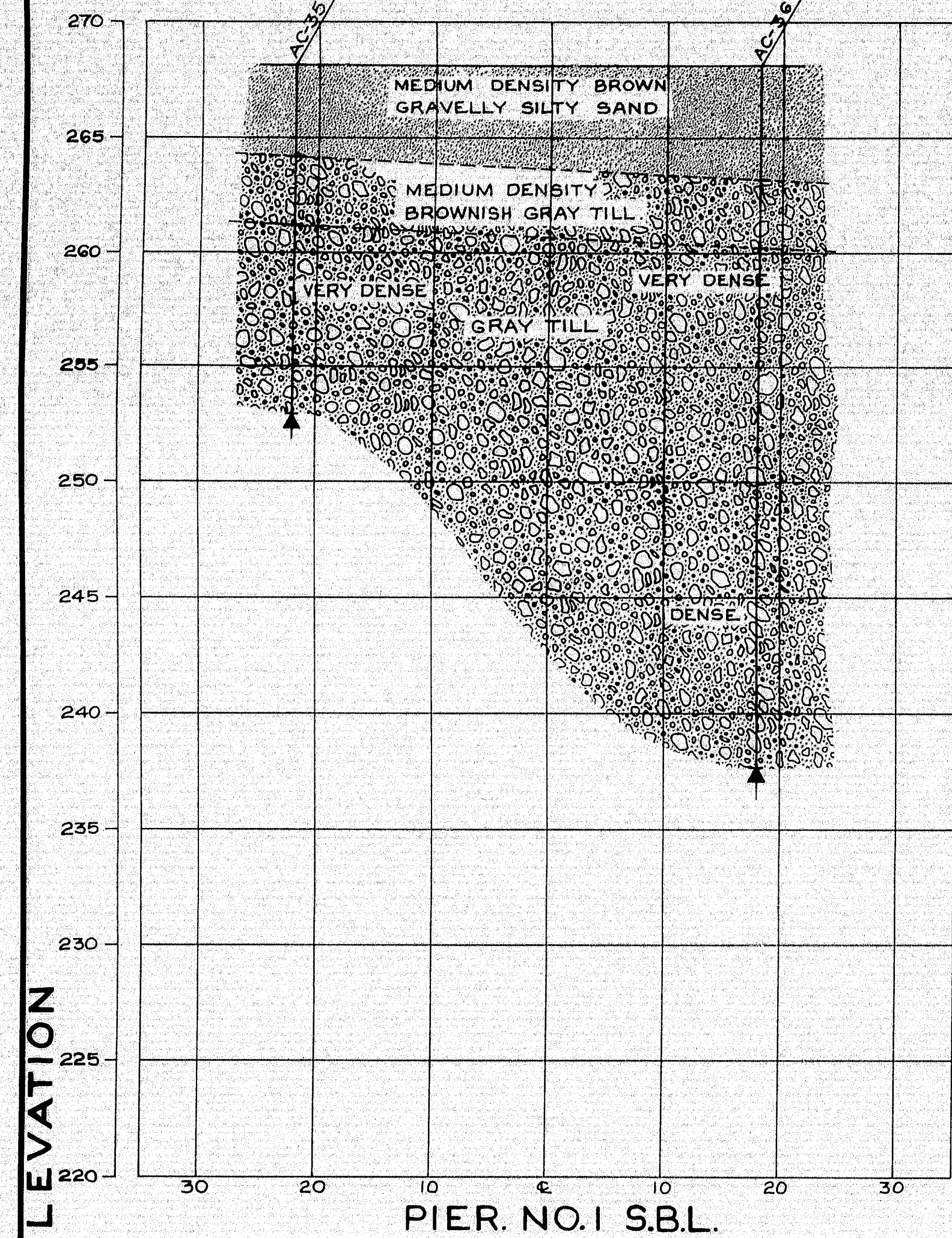


Note: Springs and artesian flow of water from wash borings were noted along the Interstate line on high ground to south of bridge location.

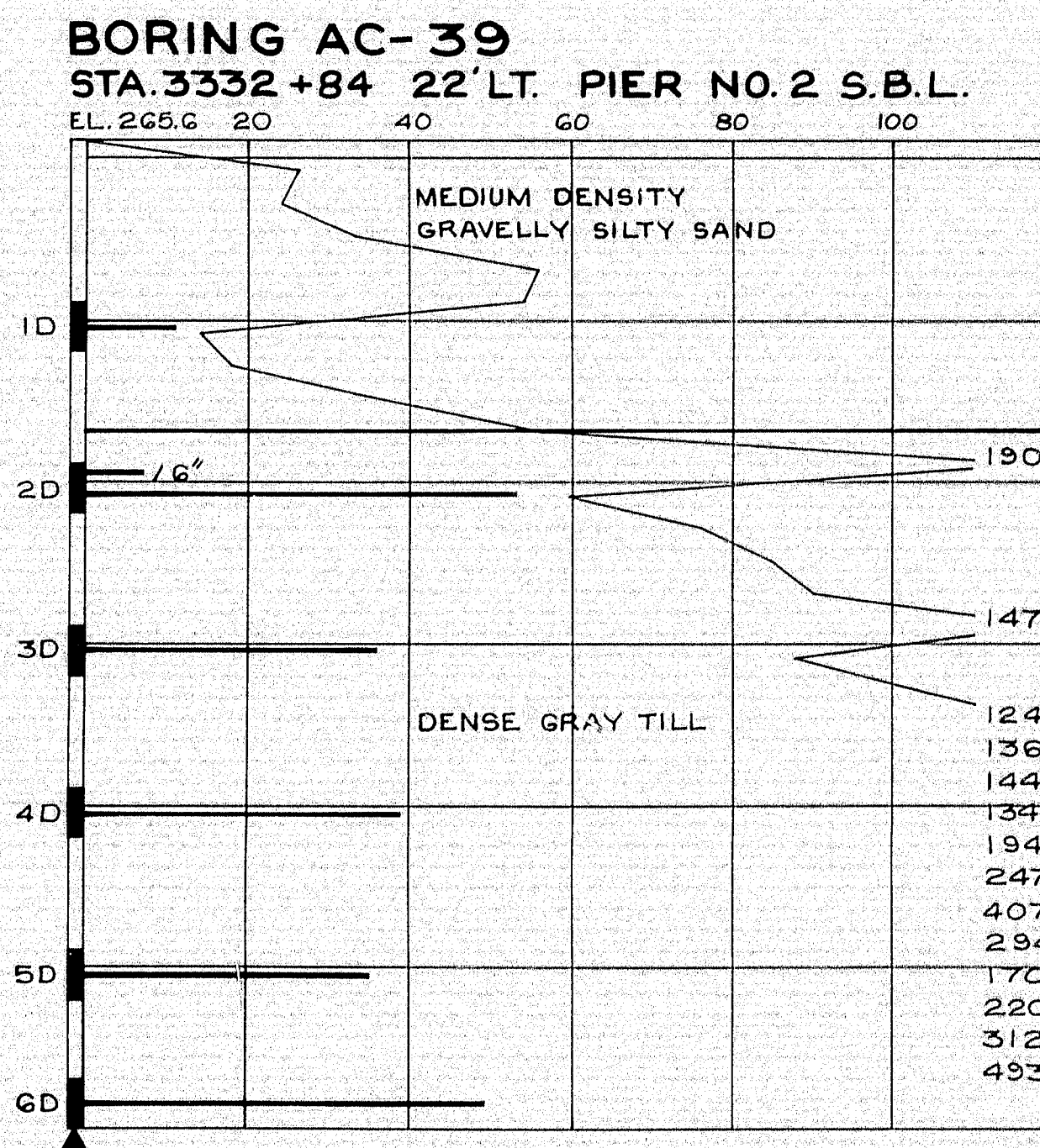
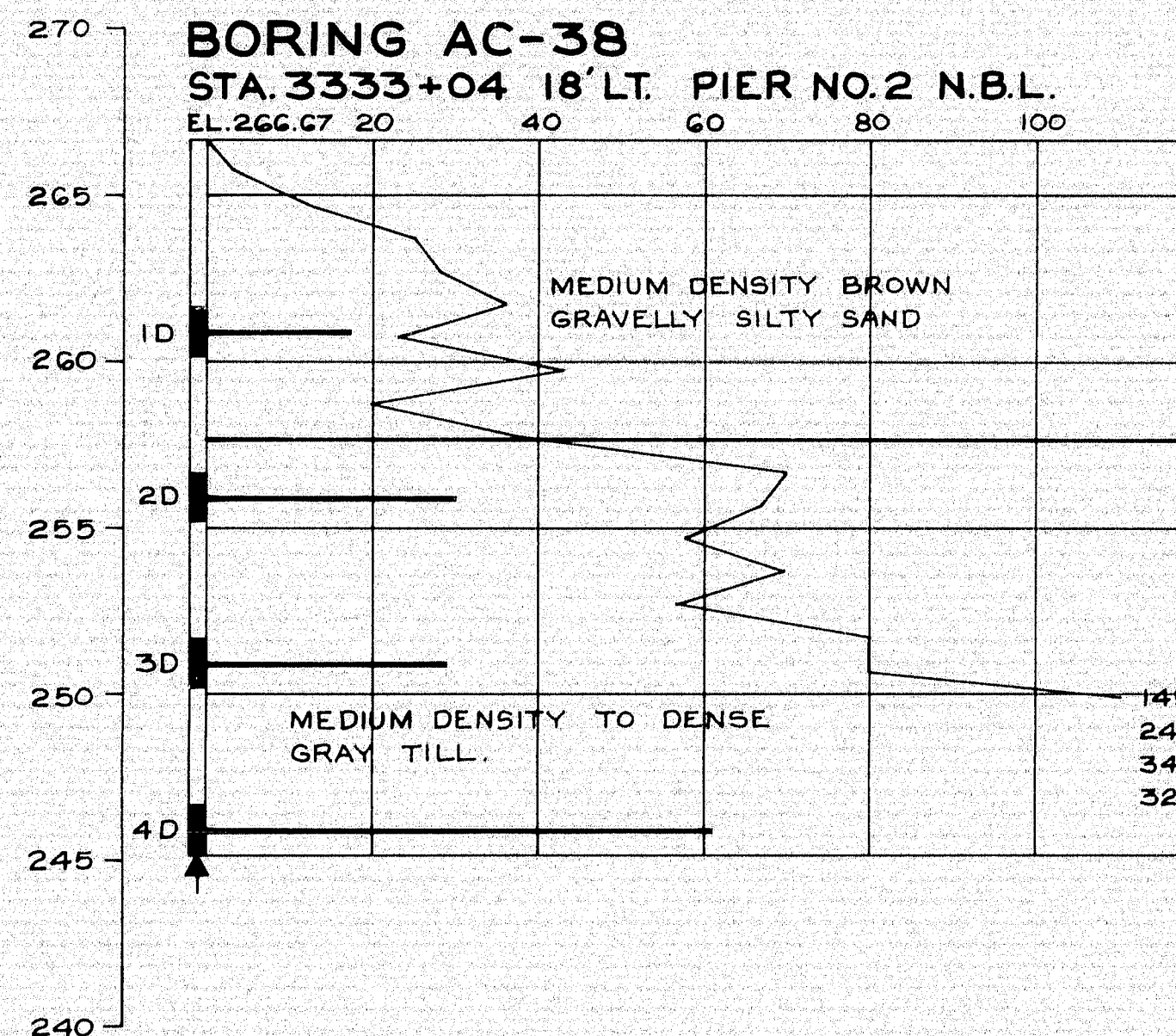
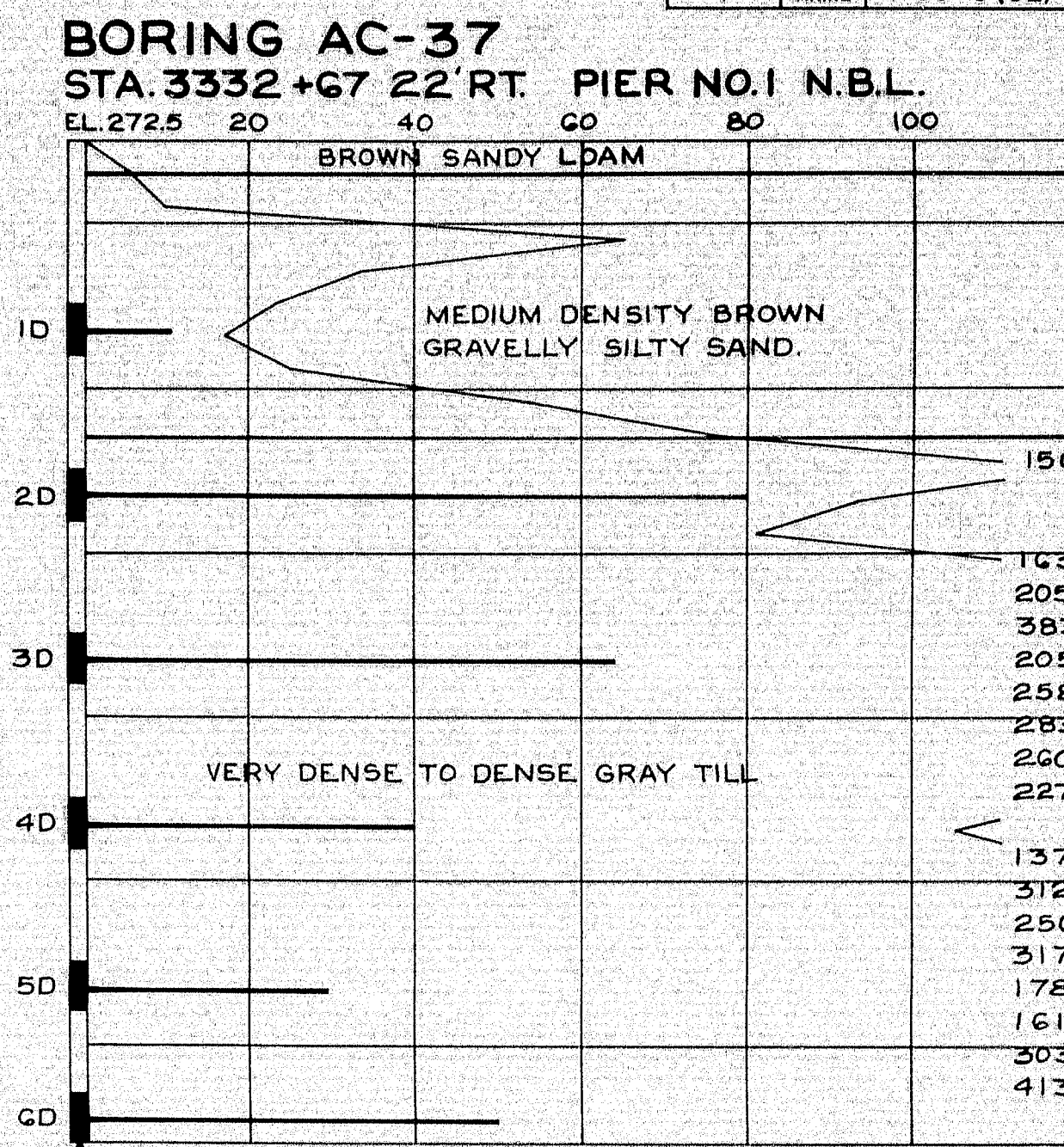
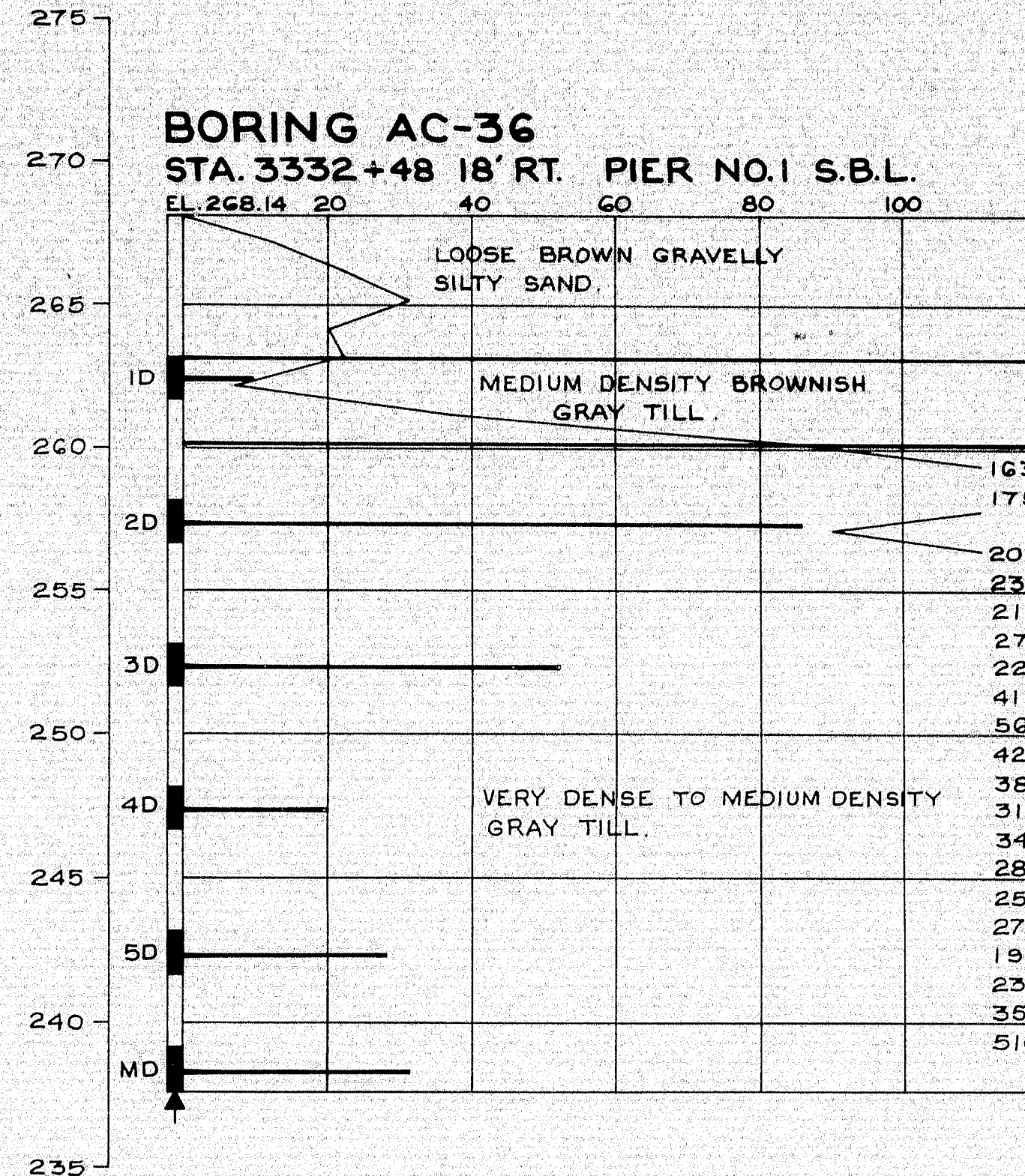
DESIGN- TRACE- CHECK-	BRIDGE NO. SURVEY- PLOT-
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95	
OVER	
ROUTE 116	
IN THE TOWN OF	
MEDWAY	
PENOBSCOT COUNTY	
FOUNDATION SURVEY	
SHEET 52 OF 93 AUGUSTA, MAINE OCT. 1964	

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TRANSVERSE PROFILES
 SCALES: HORIZ. 1"=10' VERT. 1"=5'

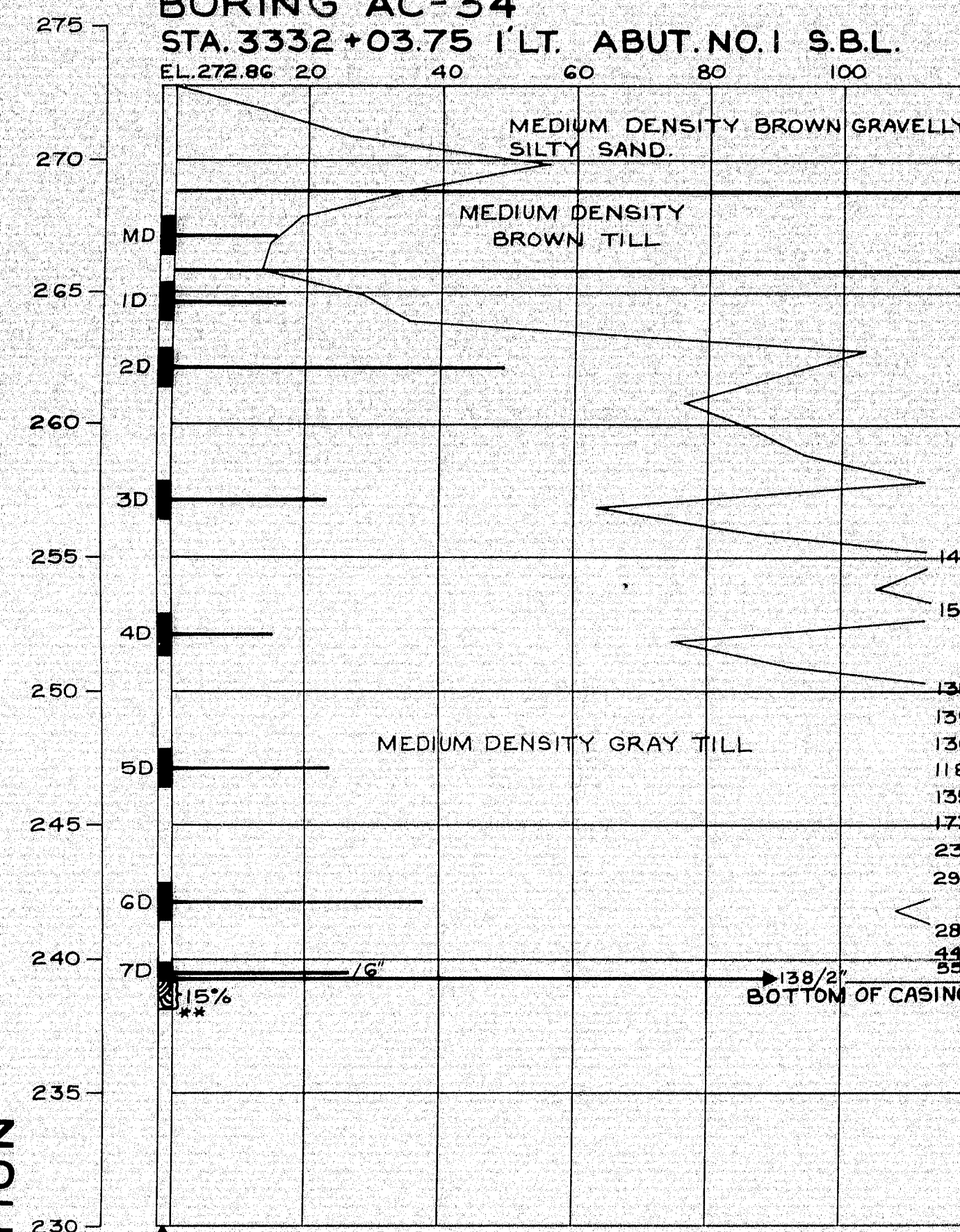


DESIGN- TRACE- CHECK-	BRIDGE NO. SURVEY- PLOT-
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95 OVER	
ROUTE 116 IN THE TOWN OF	
MEDWAY	
PENOBSCOT COUNTY	
FOUNDATION SURVEY	
SHEET 53 OF 93 AUGUSTA, MAINE OCT. 1964	

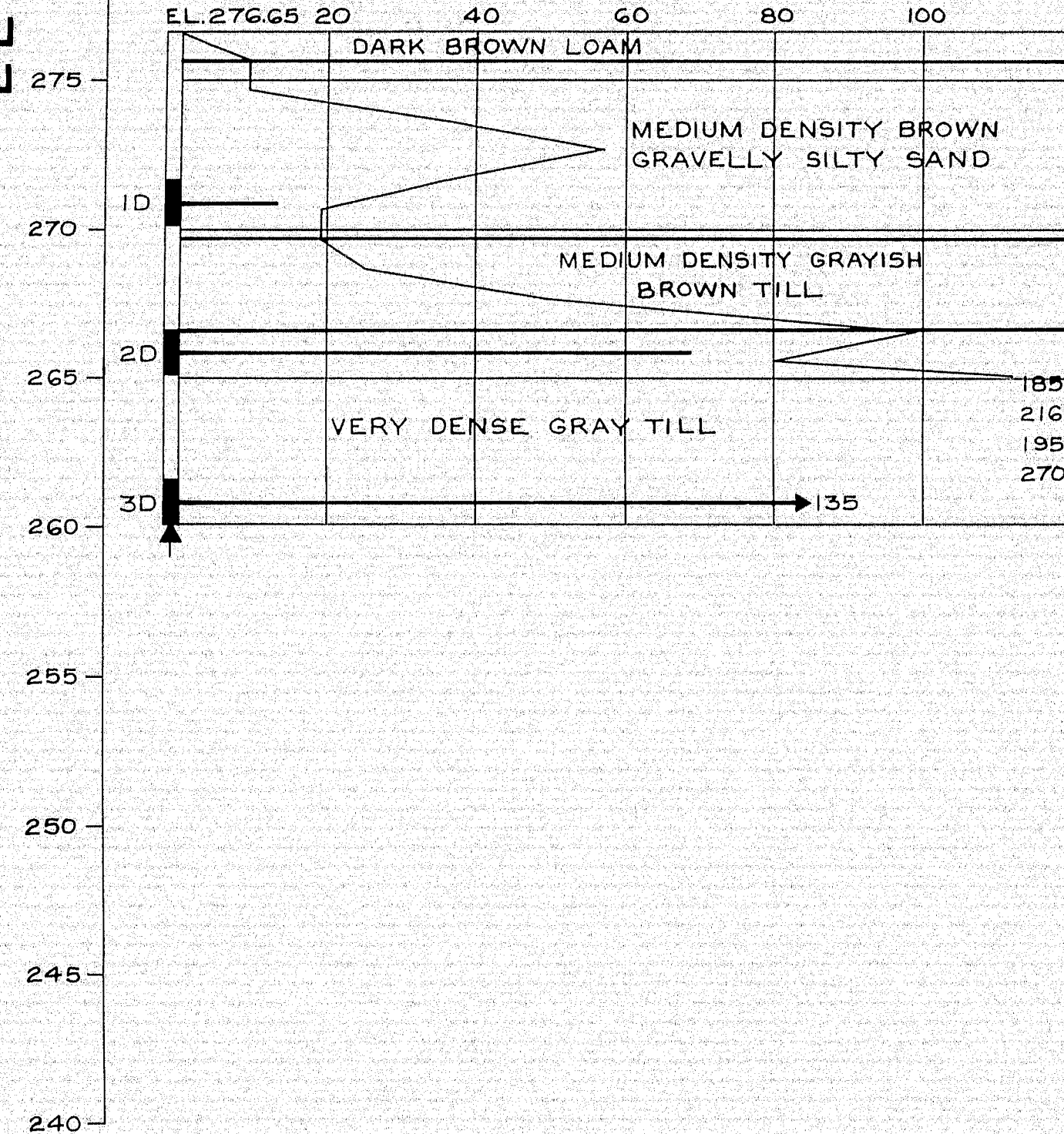
99-58



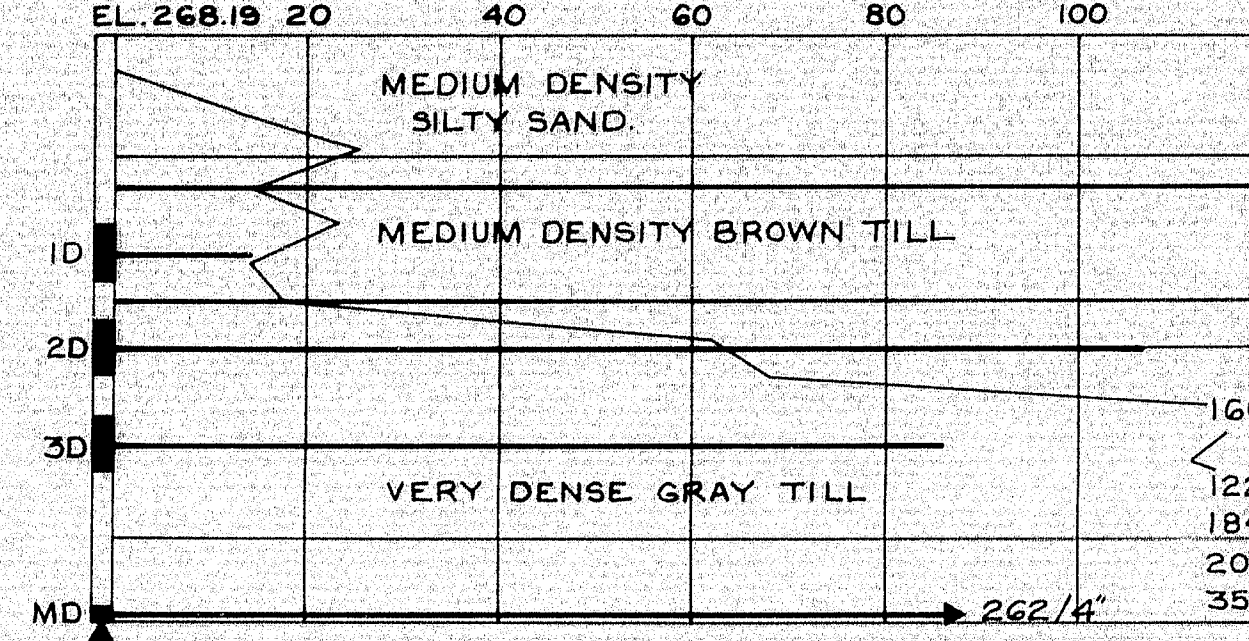
BORING AC-34
STA. 3332+03.75 I'LT. ABUT. NO. 1 S.B.L.
EL. 272.86 20 40 60 80 100



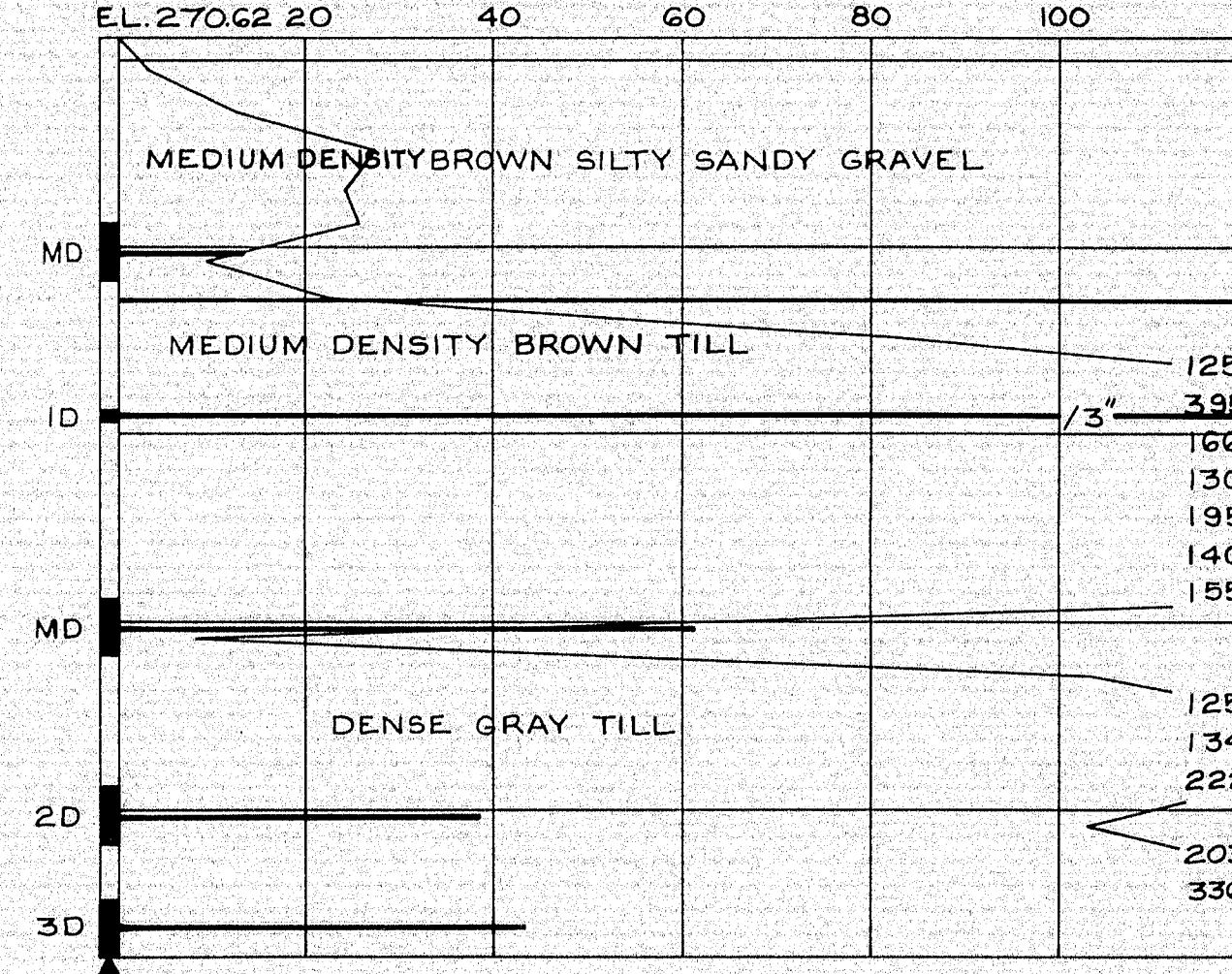
BORING AC-33
STA. 3332+23 E ABUT. NO. 1 N.B.L.
EL. 276.65 20 40 60 80 100



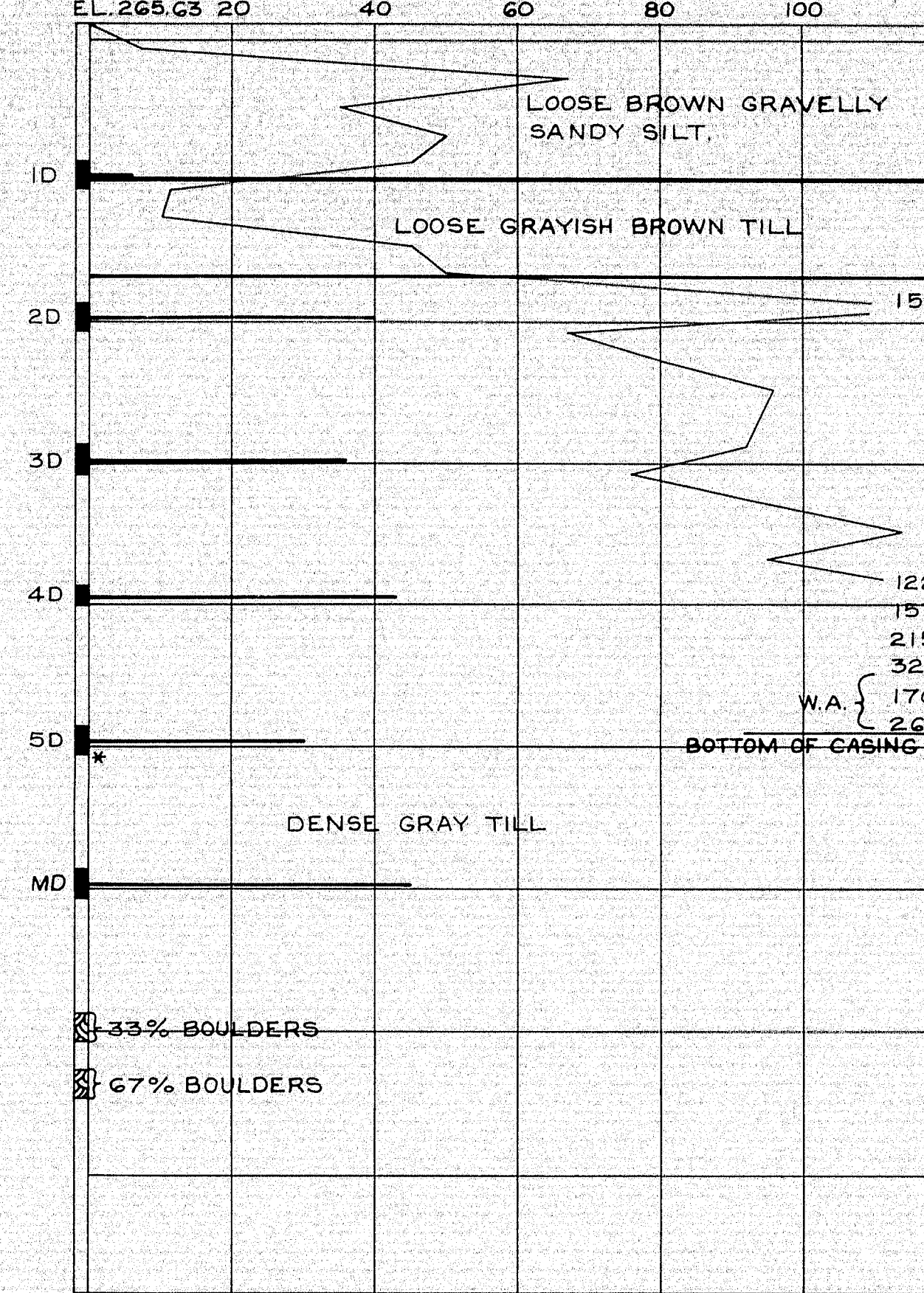
BORING AC-35
STA. 3332+42 22'LT. PIER NO. 1 S.B.L.
EL. 268.19 20 40 60 80 100



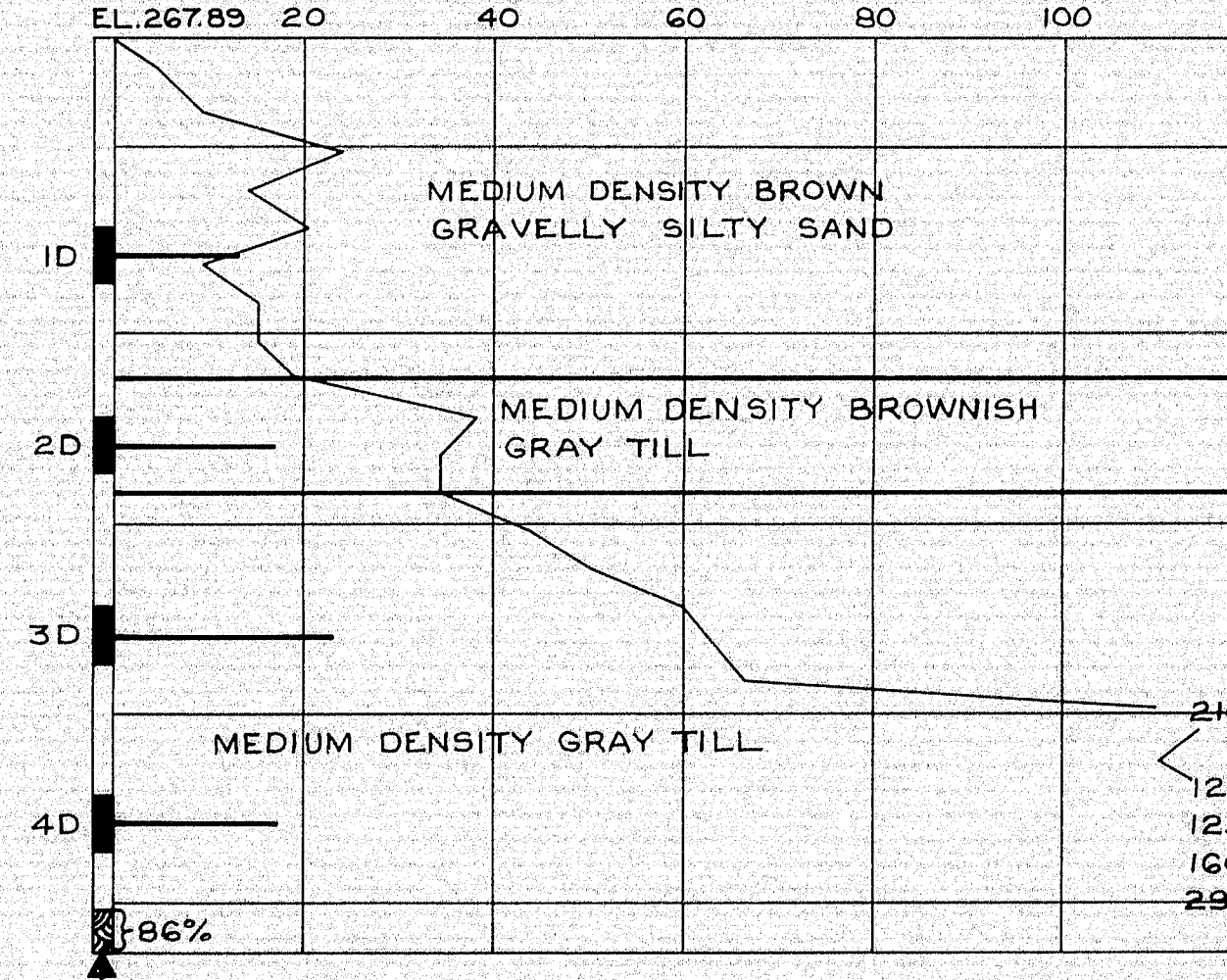
BORING AC-32
STA. 3332+61 18'LT. PIER NO. 1 N.B.L.
EL. 270.62 20 40 60 80 100



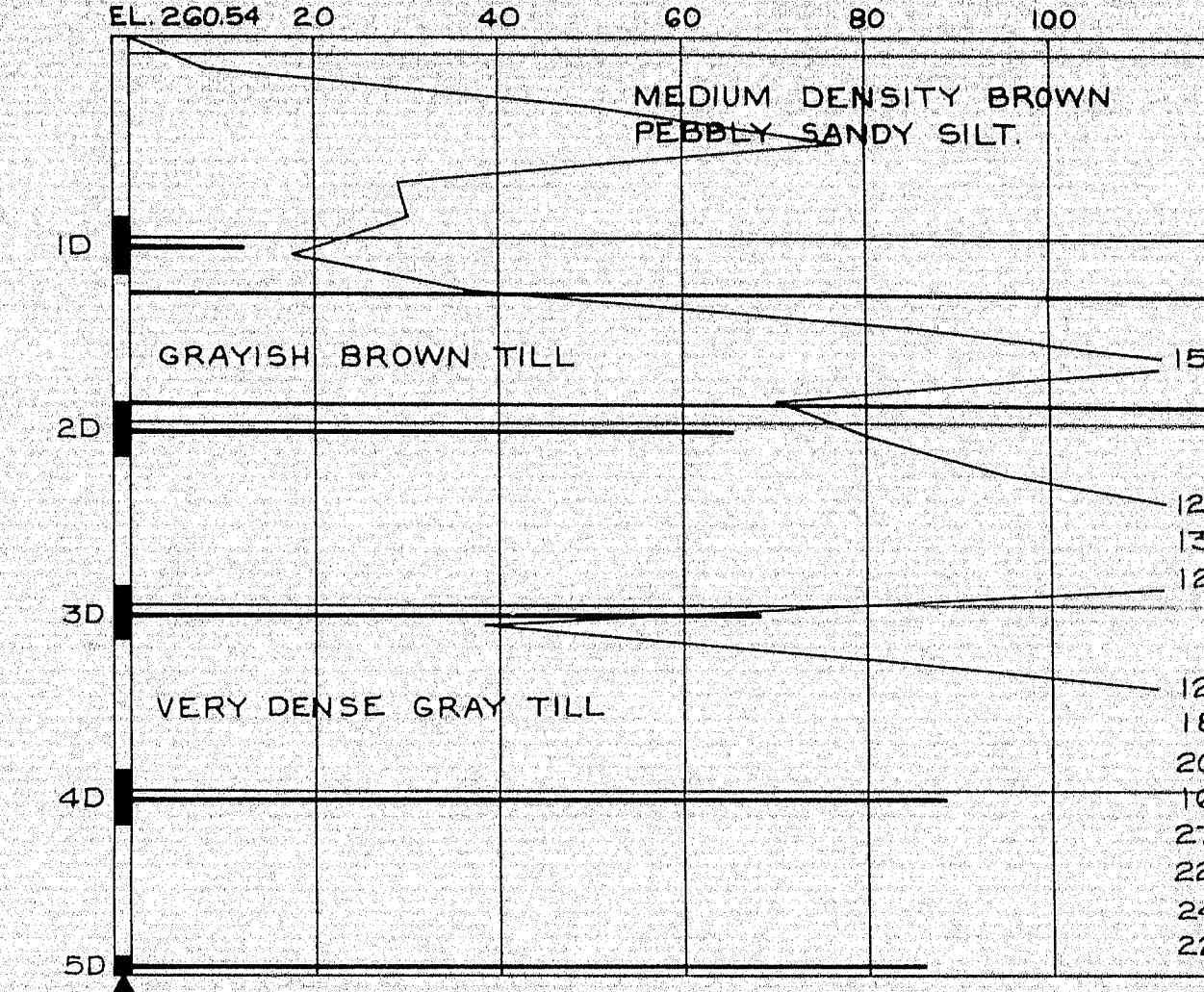
BORING AC-28
STA. 3332+90 18'RT. PIER NO. 2 S.B.L.
EL. 265.63 20 40 60 80 100



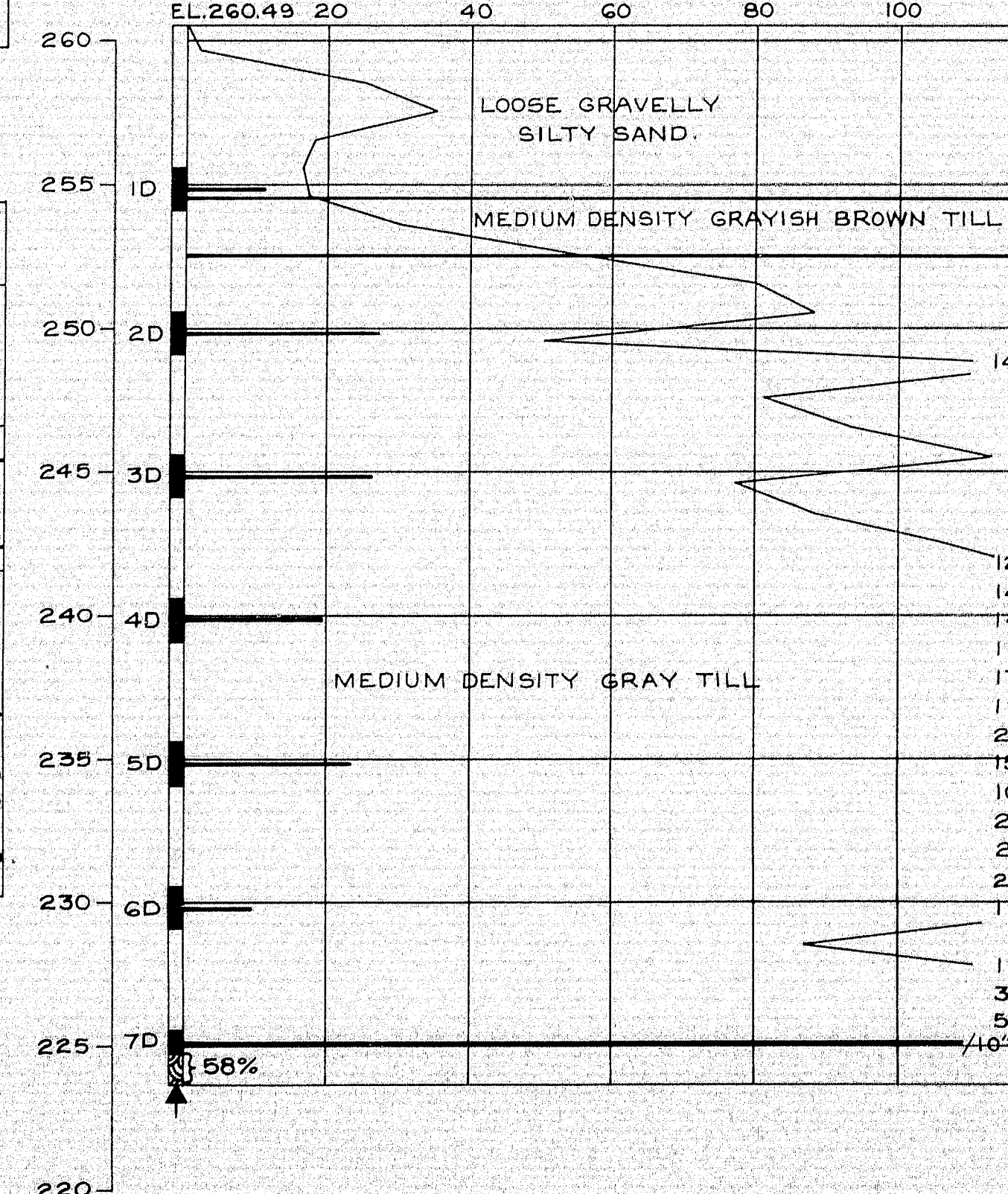
BORING AC-31
STA. 3333+10 17.5'RT. PIER NO. 2 N.B.L.
EL. 267.83 20 40 60 80 100



BORING AC-29
STA. 3333+24 E ABUT. NO. 2 S.B.L.
EL. 260.54 20 40 60 80 100



BORING AC-30
STA. 3333+44 E ABUT. NO. 2 N.B.L.
EL. 260.49 20 40 60 80 100

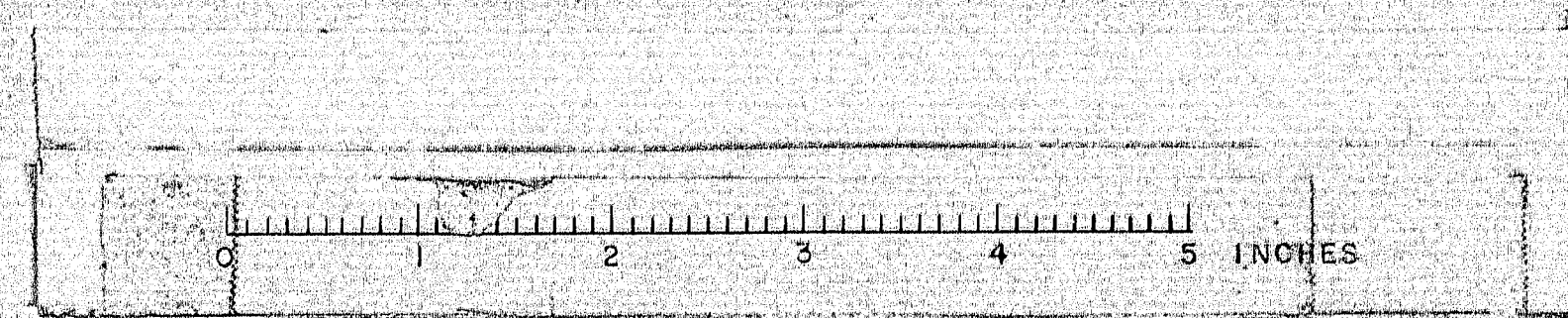


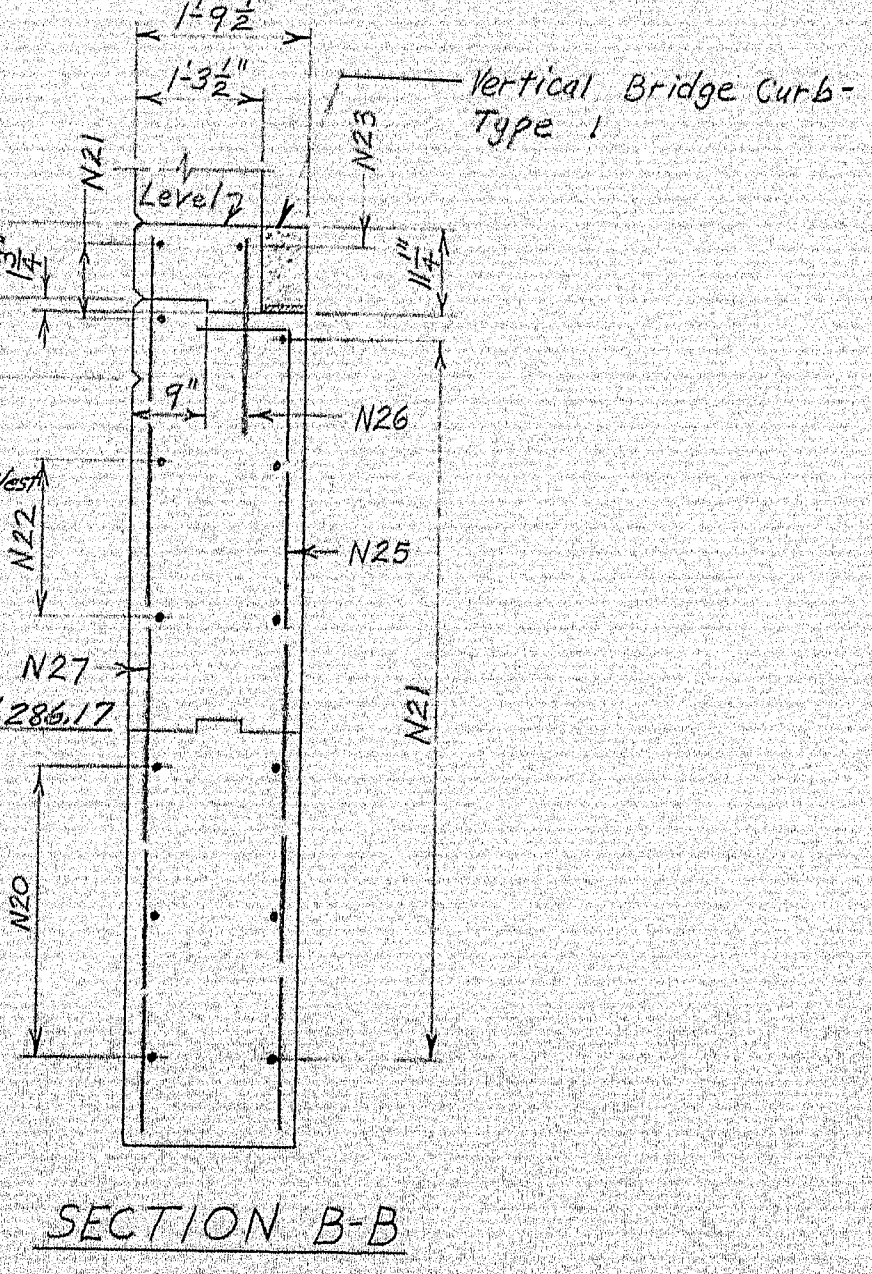
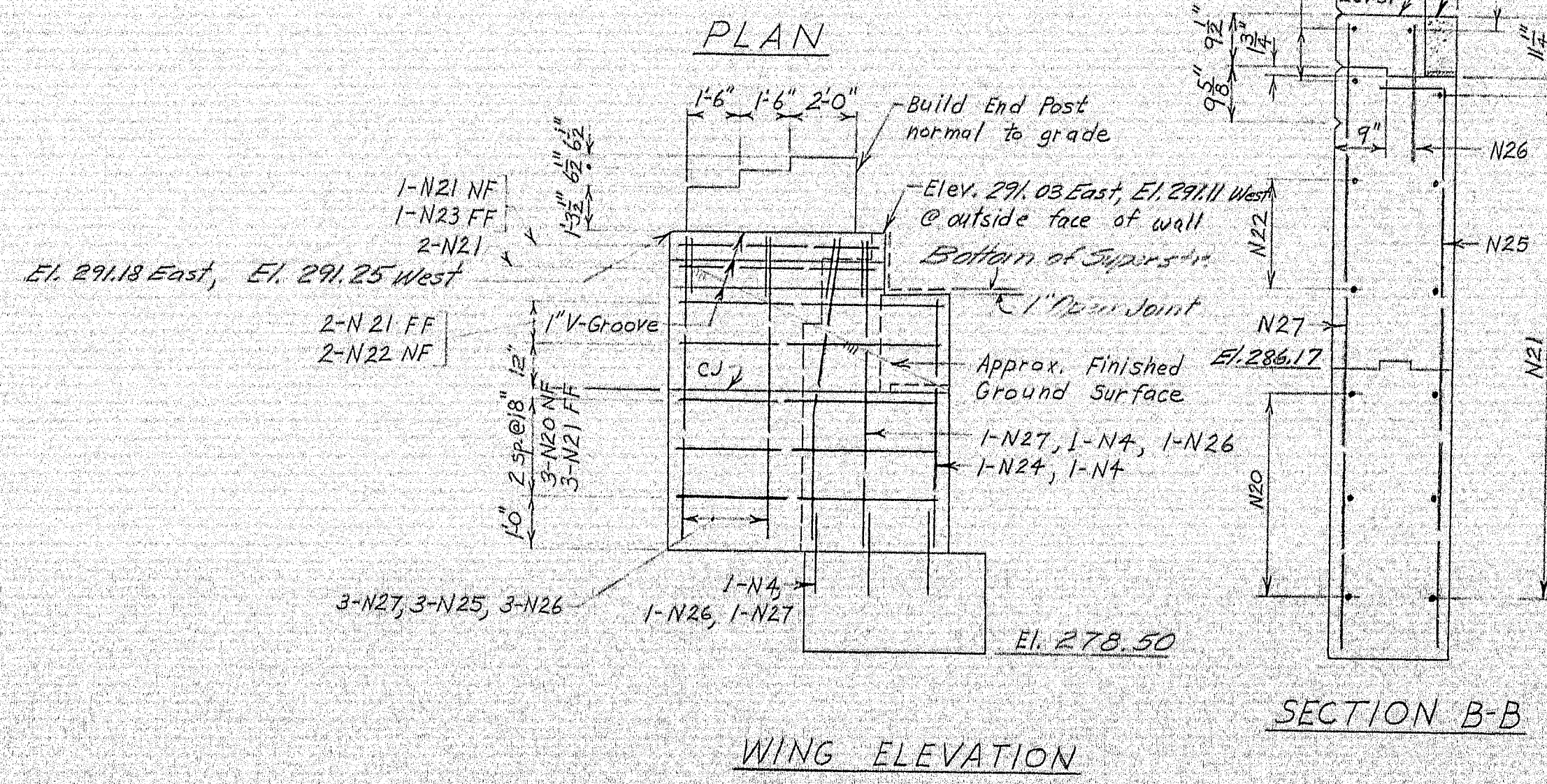
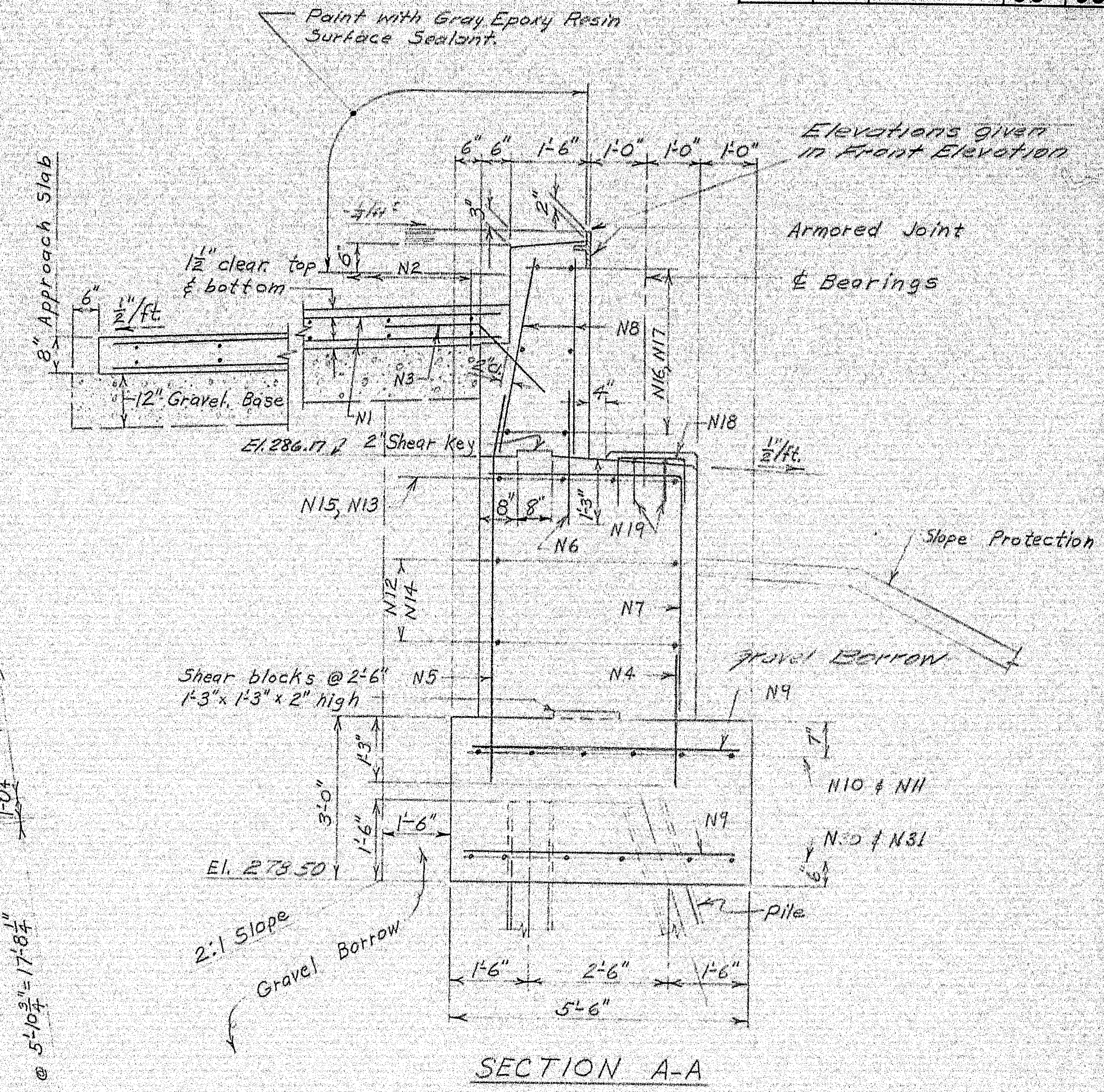
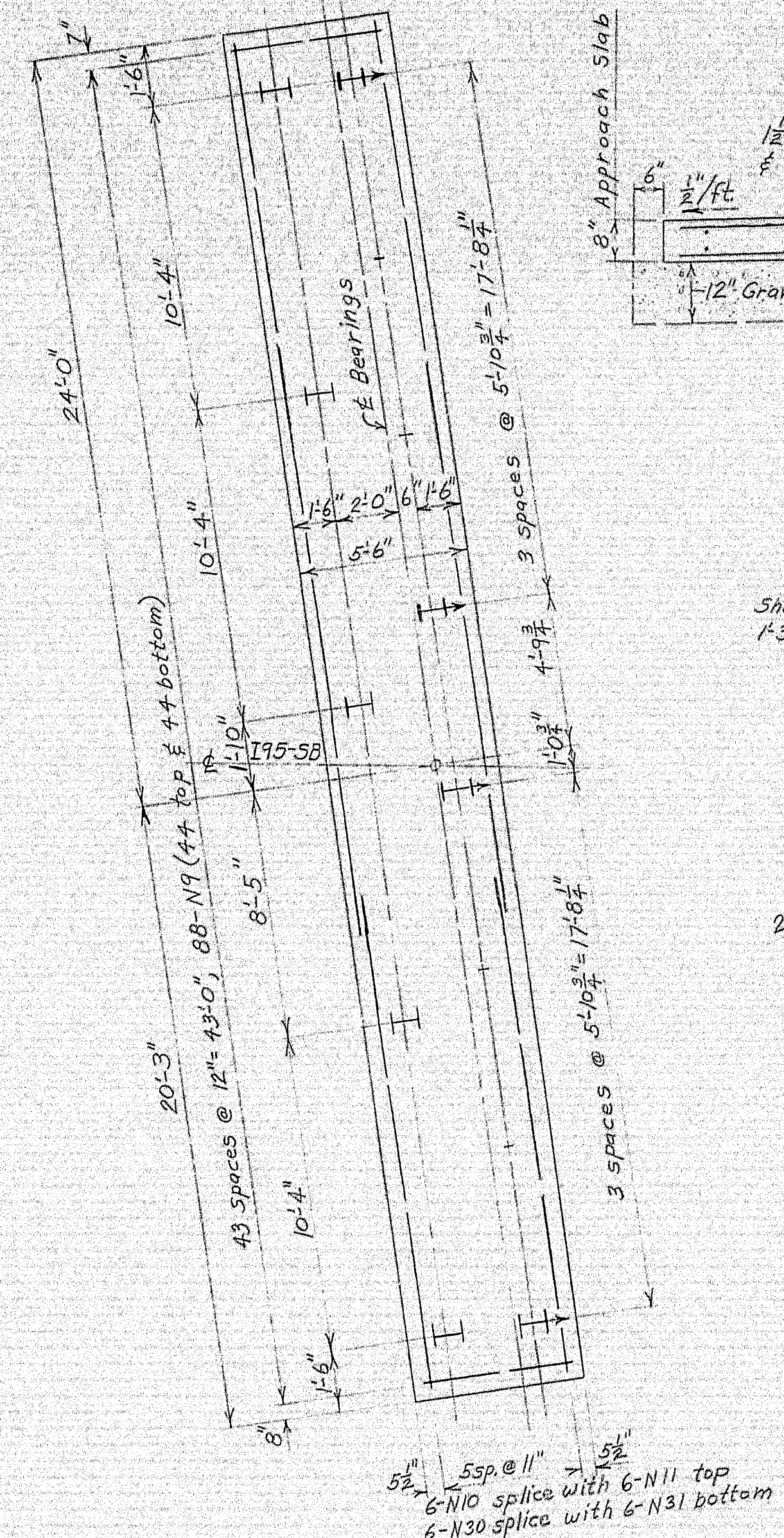
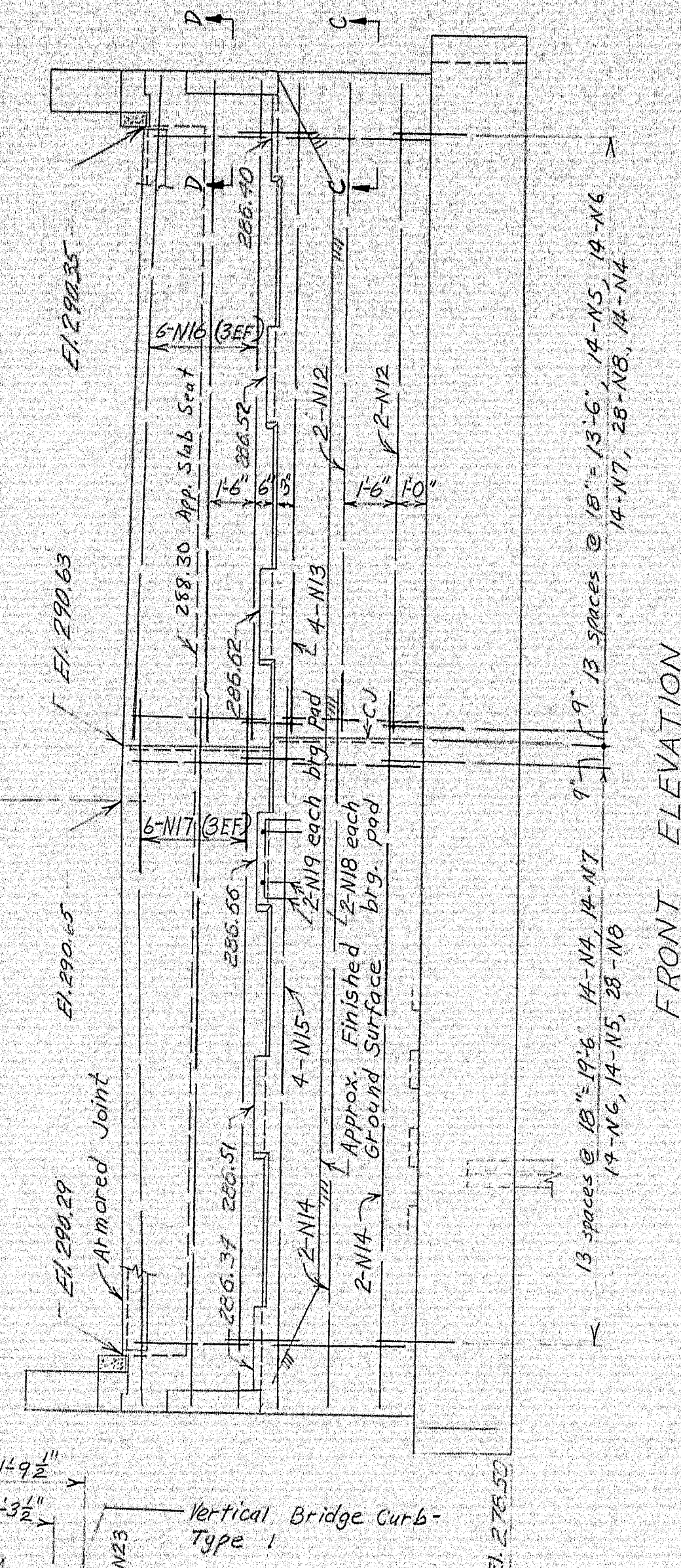
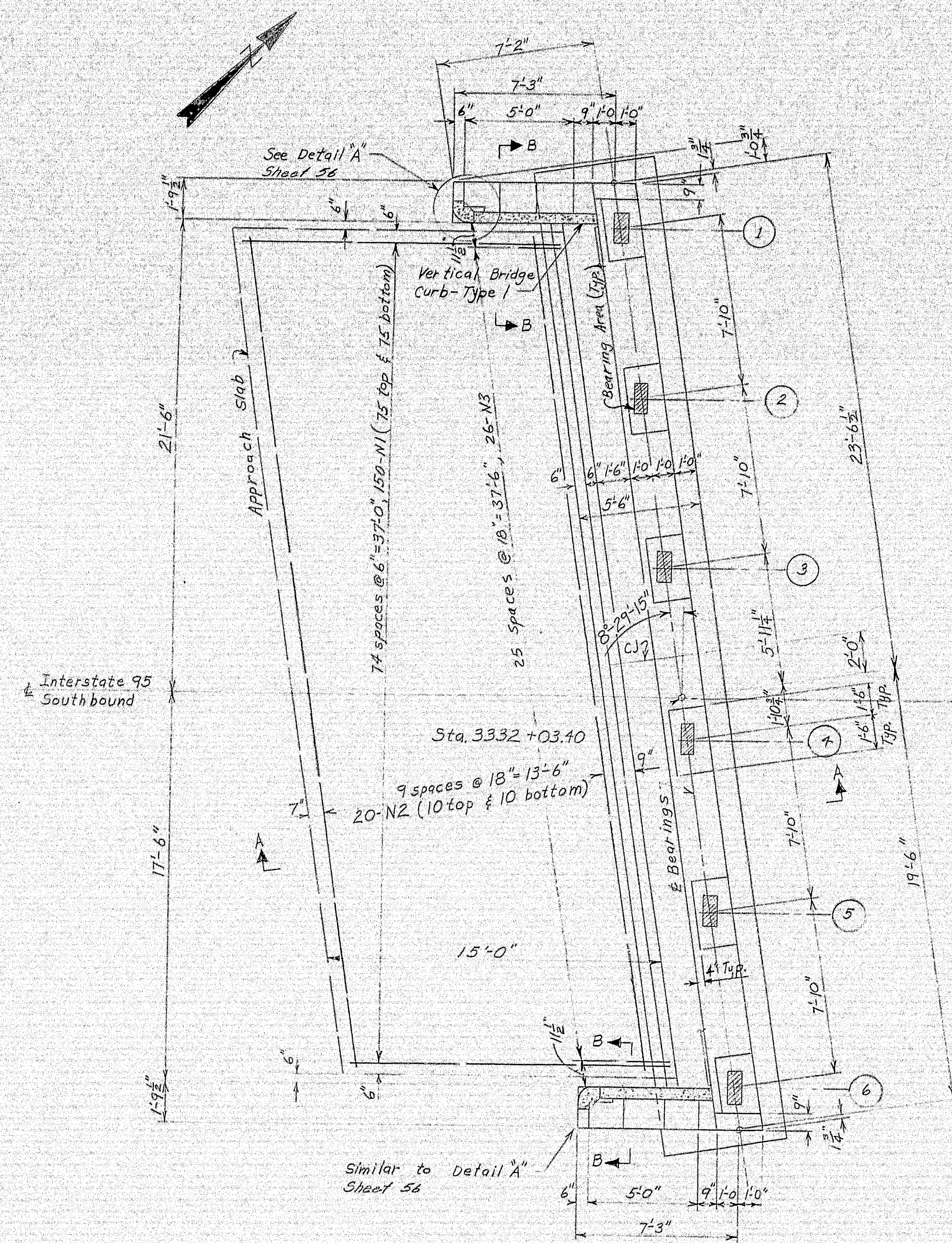
BORING NOTES

- Casing size 2 1/2"
- All samples are made ahead of casing
- Number of blows required to drive extra heavy casing one foot with 400 ft. lbs. of energy per blow.
- Location of sample or sample attempt.
- Number and type of dry sample.
- S&H Sampler #1290's
- Unsuccessful sample attempt and type of sampler.
- Number of blows required to drive spoon or tubing one foot with 350 ft. lbs. of energy per blow.
- Bottom of boring (may not be bottom of soil strata)
- Locations cored by diamond bit and per cent recovery of rock.
- * Drilled ahead with hard faced bit.
- ** Drilled ahead with pod auger.
- WA. Washed ahead

DESIGN- TRACE- CHECK-	BRIDGE NO. SURVEY- PLOT-
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95	
OVER	
ROUTE 116	
IN THE TOWN OF	
MEDWAY	
PENOBSCOT COUNTY	
BORING DETAILS	
SHEET 54 OF 93 AUGUSTA, MAINE OCT. 1964	

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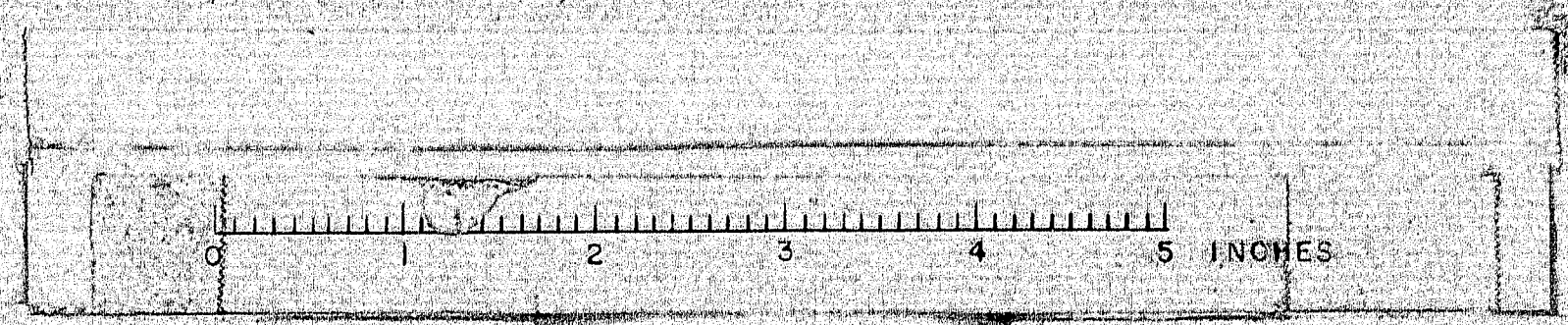


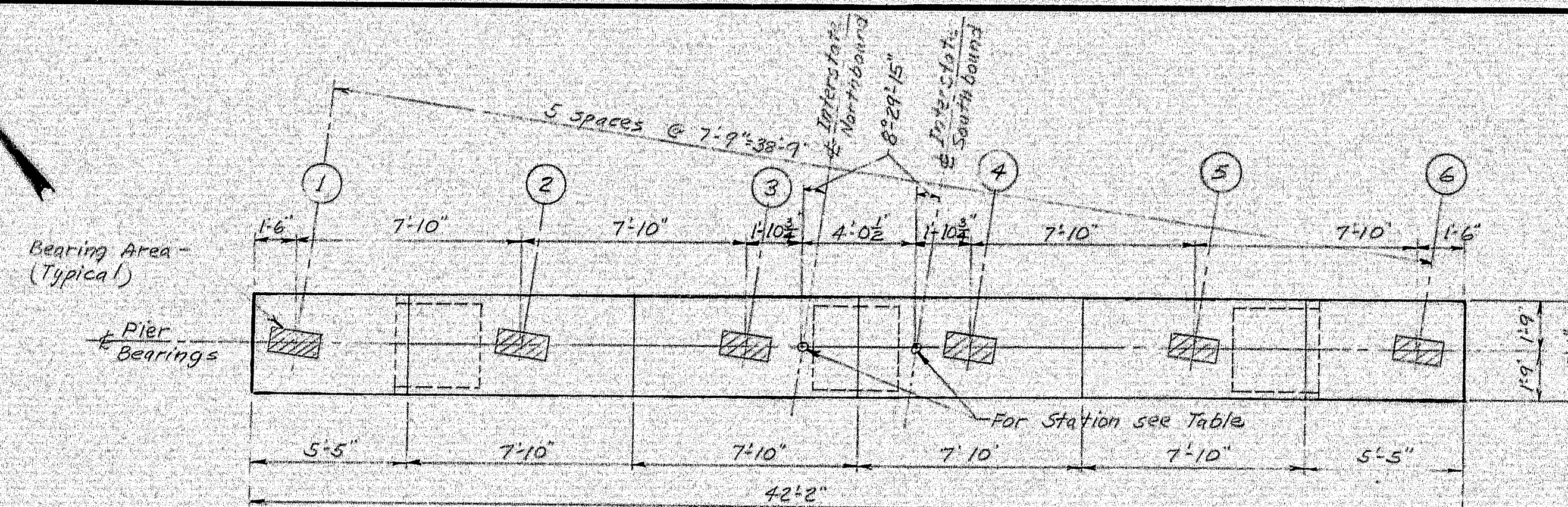
DESIGN - CDH
TRACE - LLS
CHECK - WRA

BRIDGE NO.
SURVEY
PLOT

STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95
OVER
ROUTE 116
IN THE TOWN OF
MEDWAY
PENOBSCOT COUNTY
ABUTMENT 1 SOUTHBOUND
SHEET 55 OF 93 AUGUSTA, MAINE OCTOBER 1964

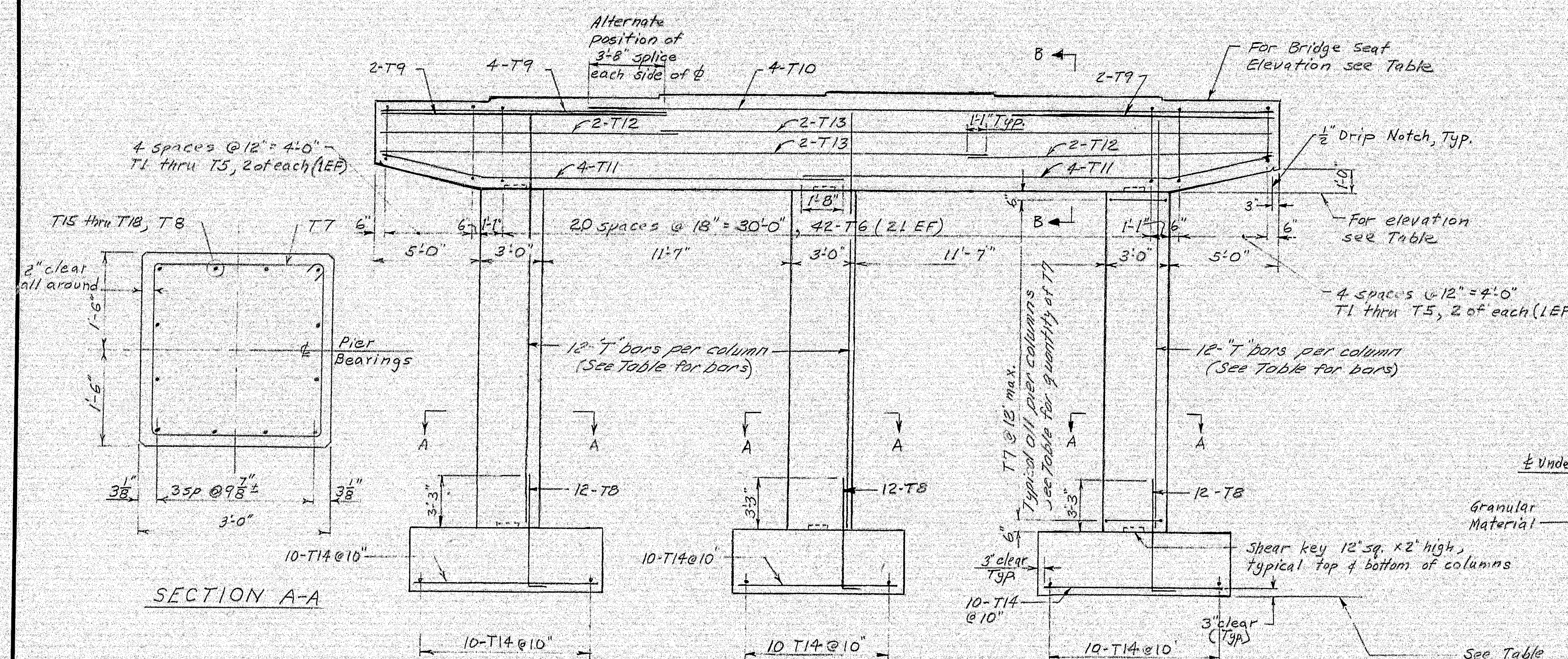
99-60



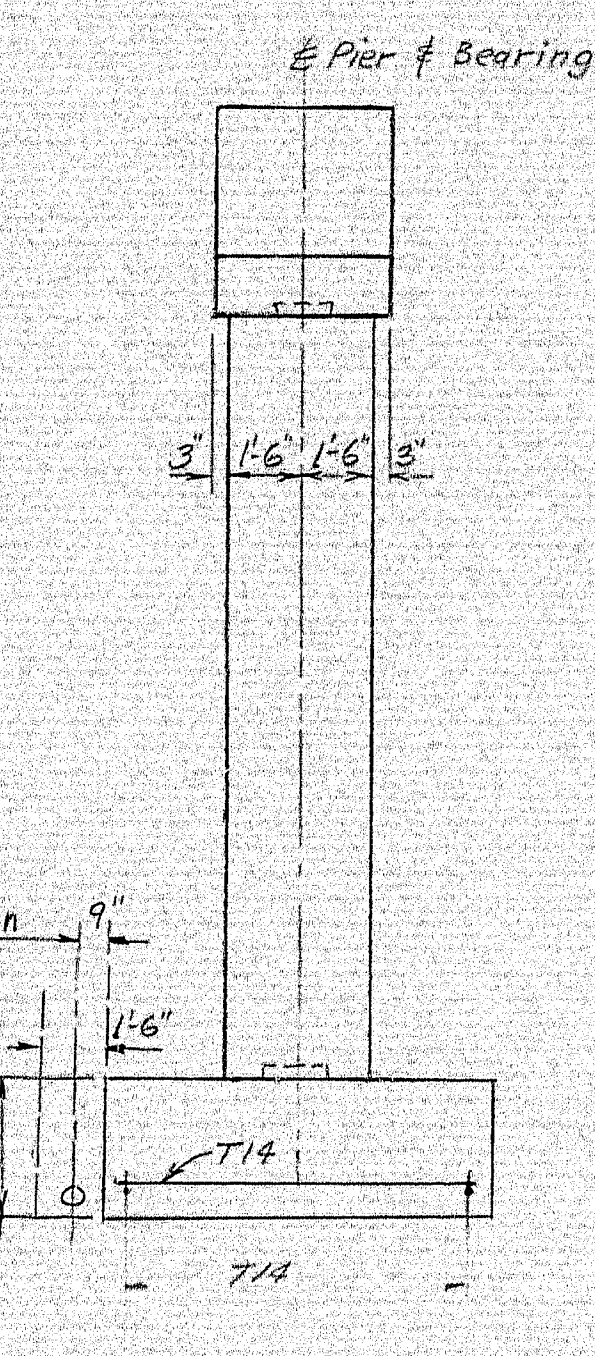


PLAN

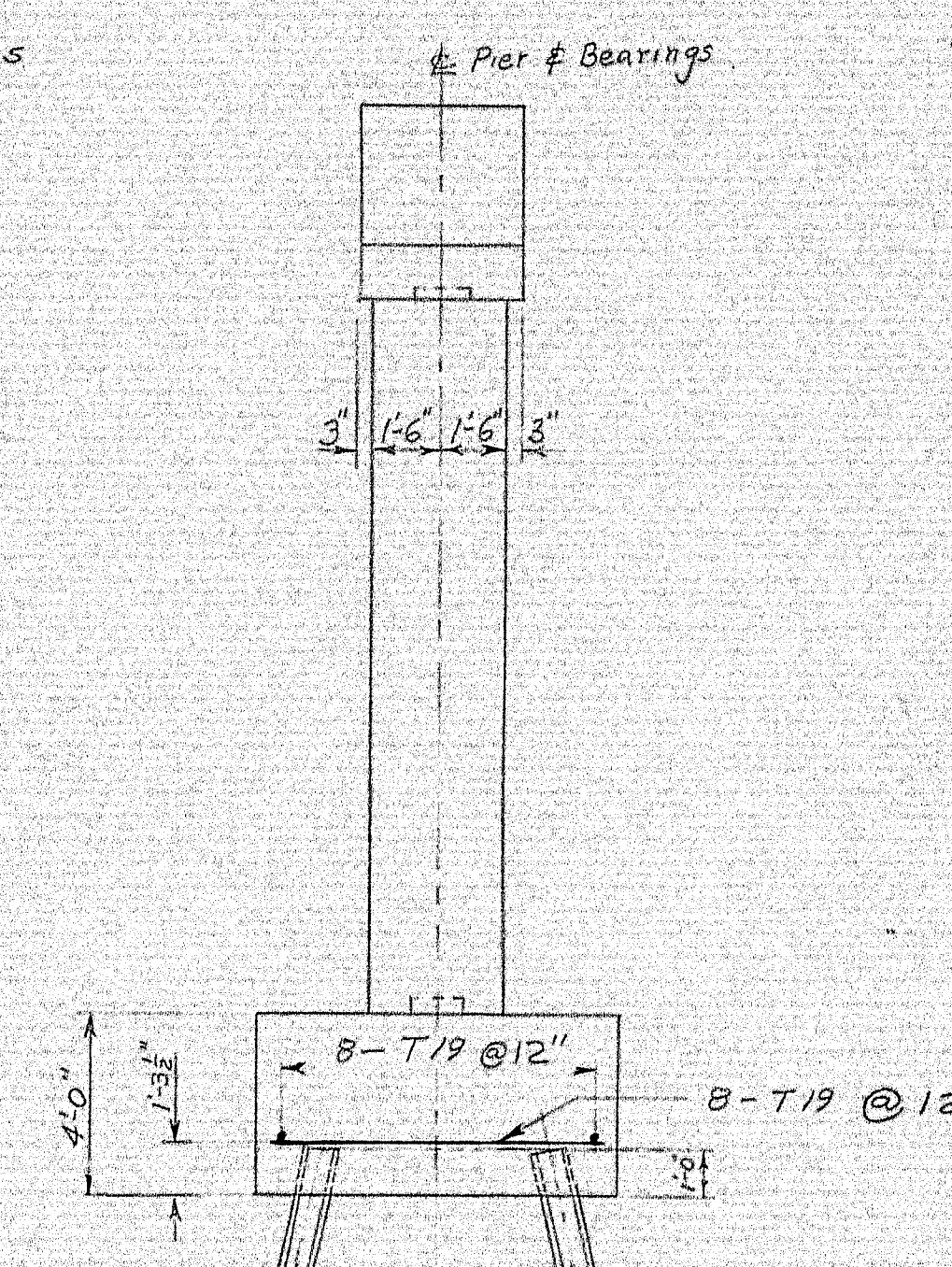
PIER	STATION ± Pier & ± Interstate	BRIDGE SEAT ELEVATIONS						Bottom of Cap Elevation	Bottom of Footing Elevation	Column Length	Column Reinf.
		(1)	(2)	(3)	(4)	(5)	(6)				
#1-SB	3332+44.90	285.50	285.61	285.72	285.76	285.60	285.44	281.44	260.00	18.44	19-T7 12-T15
#2-SB	3332+87.40	284.65	284.76	284.87	284.92	284.76	284.61	280.61	260.00	15.11	16-T7 12-T16
#1-NB	3332+64.30	285.79	285.90	286.01	285.91	285.75	285.59	281.59	260.00	17.59	18-T7 12-T17
#2-NB	3333+06.80	284.90	285.01	285.12	285.02	284.87	284.71	280.71	260.00	14.71	15-T7 12-T18



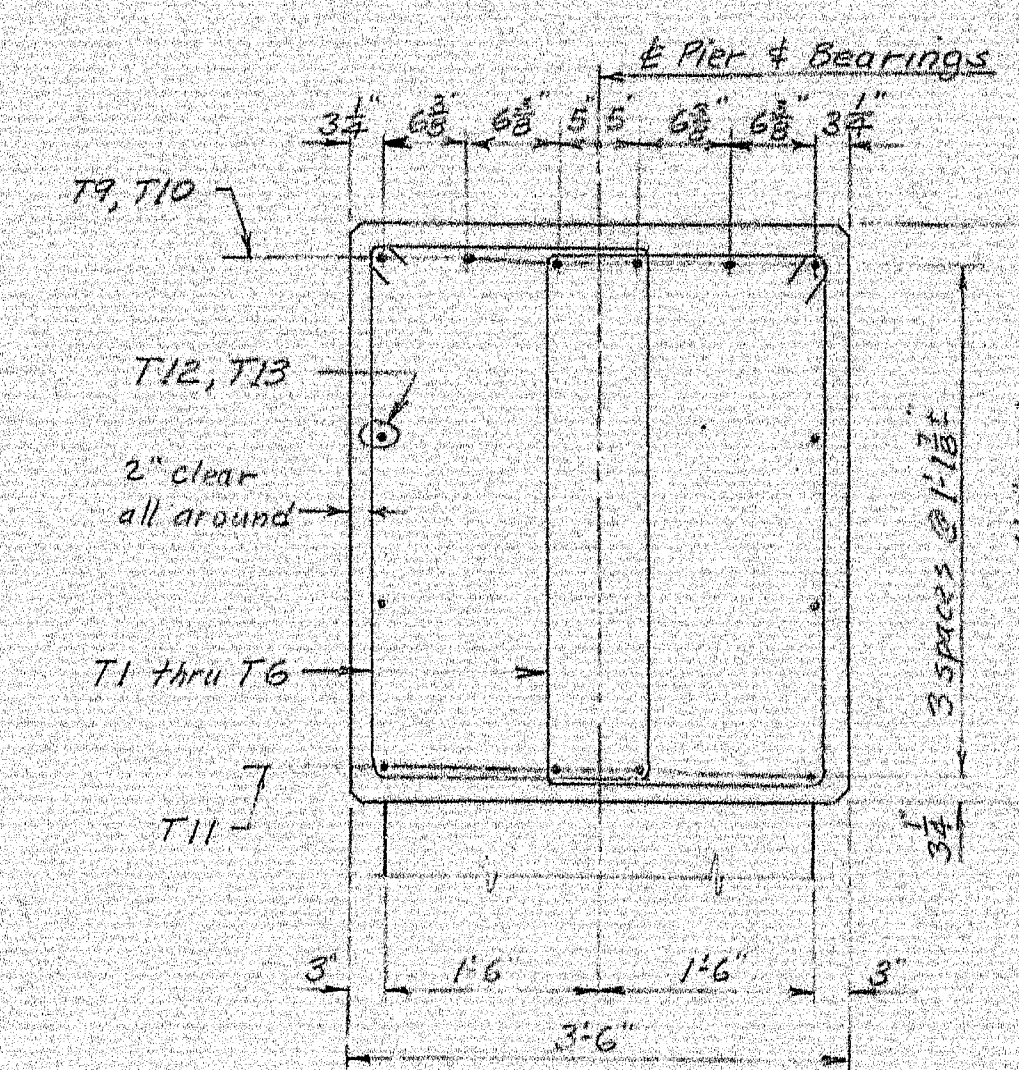
FRONT ELEVATION



END ELEVATION
PIERS #1



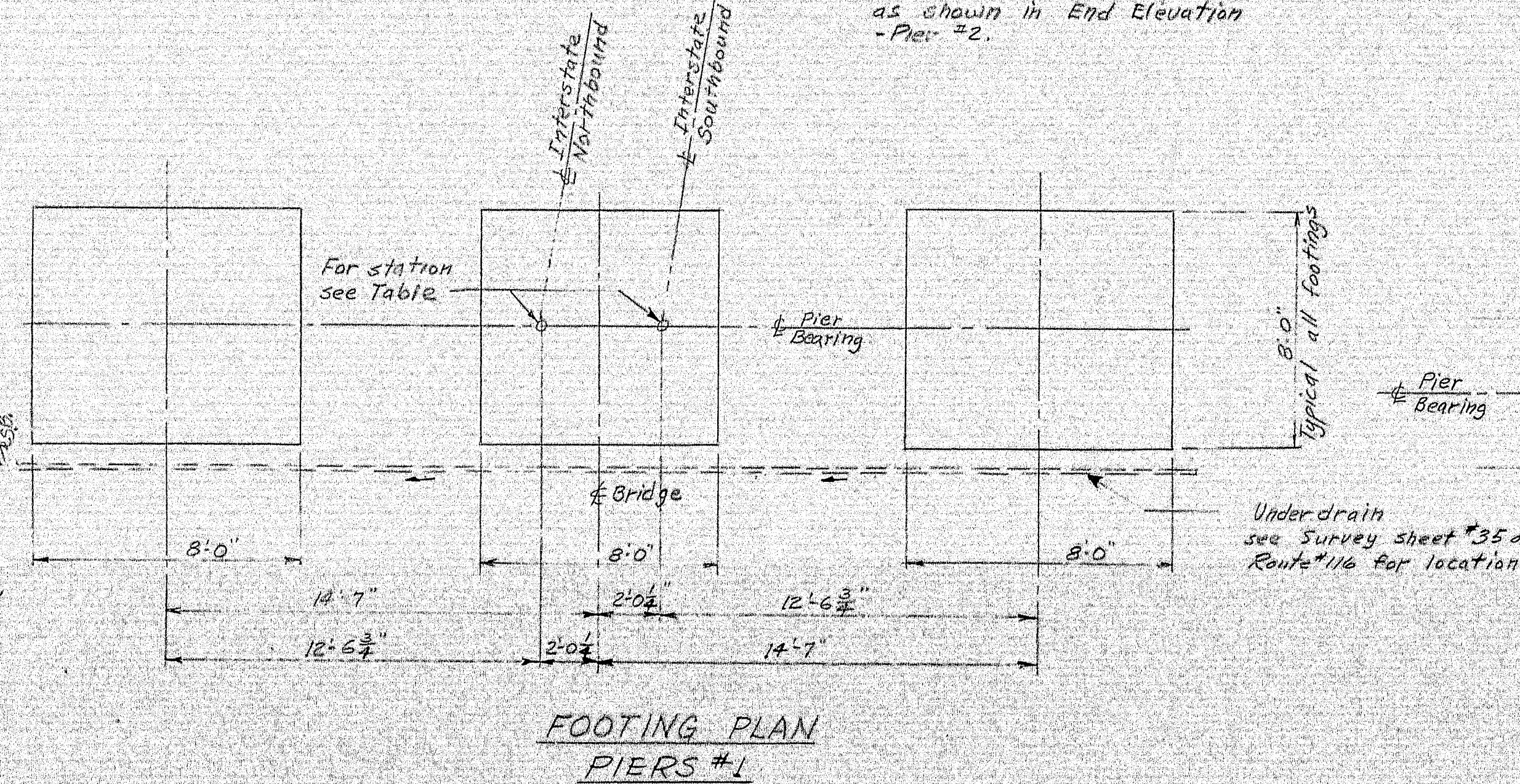
END ELEVATION
PIERS #2



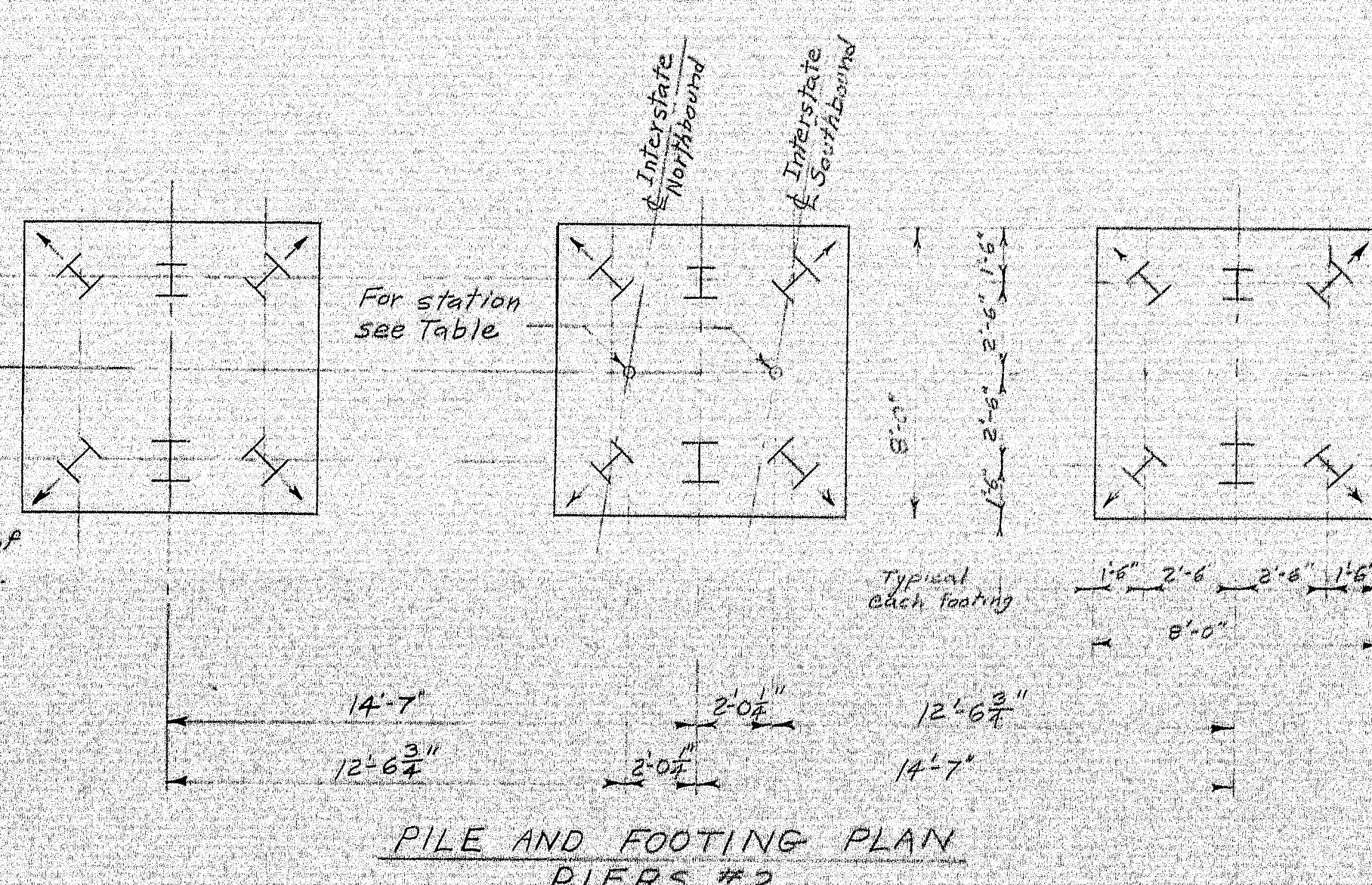
SECTION B-B

PILE NOTES

- All piles shall be 10BP42 steel H-piles.
- Maximum Pile Load = 37 tons.
- Piles marked thus $H \rightarrow$ shall be battered $2\frac{1}{2}$ inches per foot in direction of arrow.
- Piles shall be driven to ledge or practical refusal to develop end bearing.
- Estimated length of piles:
Pier #2-SB 18 @ 22 feet
Pier #2-NB 18 @ 19 feet
- See Pile Point Detail Sheet 57.



FOOTING PLAN
PIERS #1



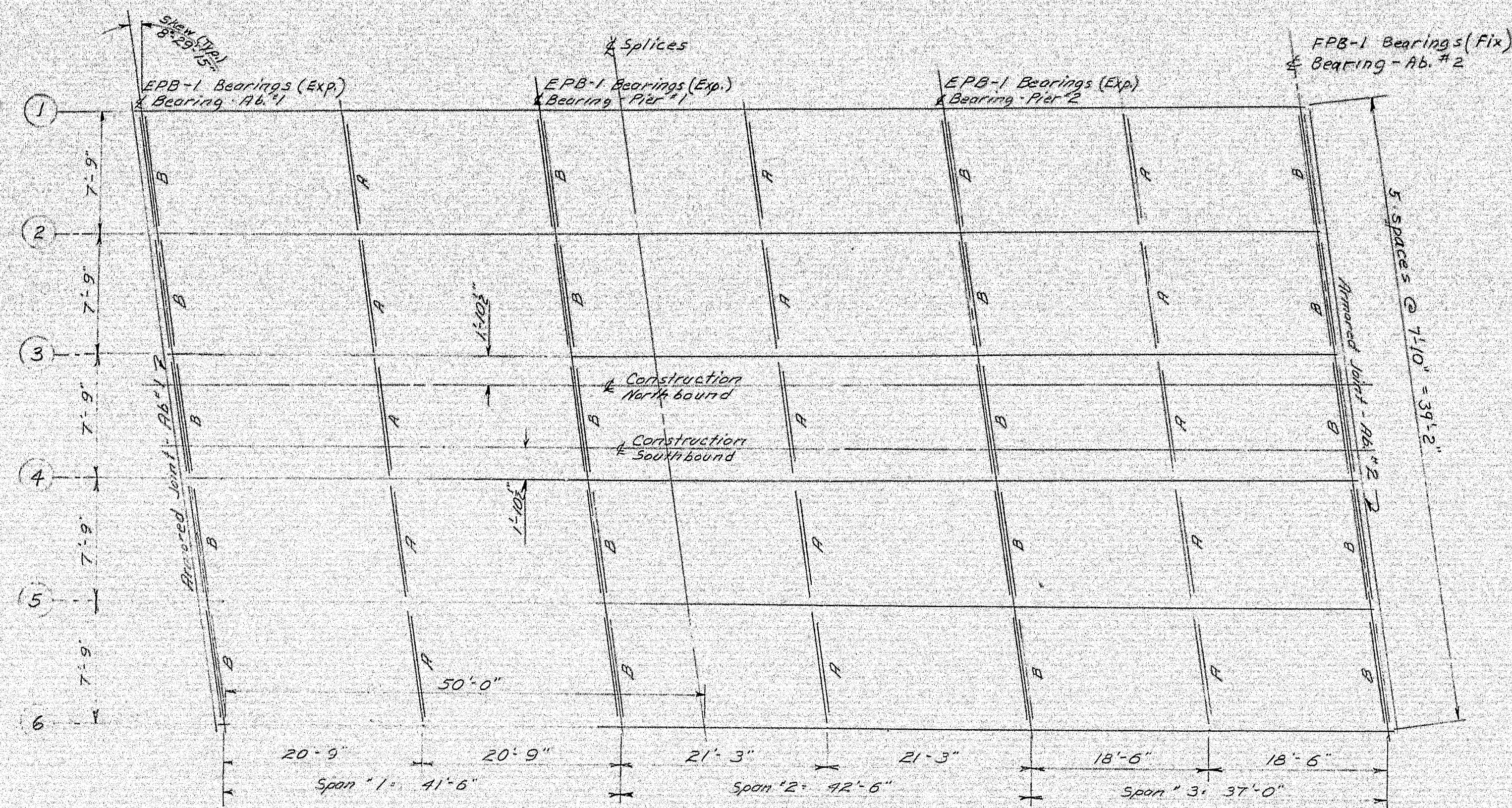
PILE AND FOOTING PLAN
PIERS #2

PIER NOTES

- Place reinforcing steel in bridge seats to clear swaged anchor bolts.
- See Standard Detail sheet 8D101-64, Bearing Pedestals for note concerning preparation of Bearing Areas.
- Abbreviations:
SB = South bound
NB = North bound
EF = Each Face
- Maximum Footing Pressure - Piers #1
Group I 2.0 tons per sq. ft.
Group VI 6.2 tons per sq. ft.

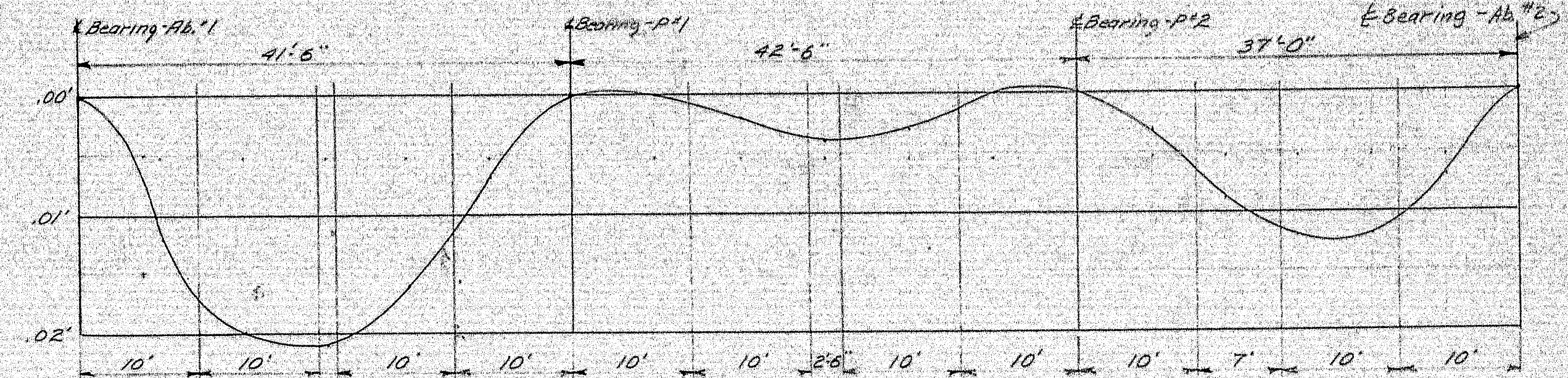
DESIGN - EBN TRACE - HQ CHECK - HQ	BRIDGE NO. SURVEY PILOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95 OVER	
ROUTE 116 IN THE TOWN OF	
MEDWAY	
PENOBSCOT COUNTY	
PIERS	
SHEET 59 OF 93 AUGUSTA, MAINE OCTOBER 1964	

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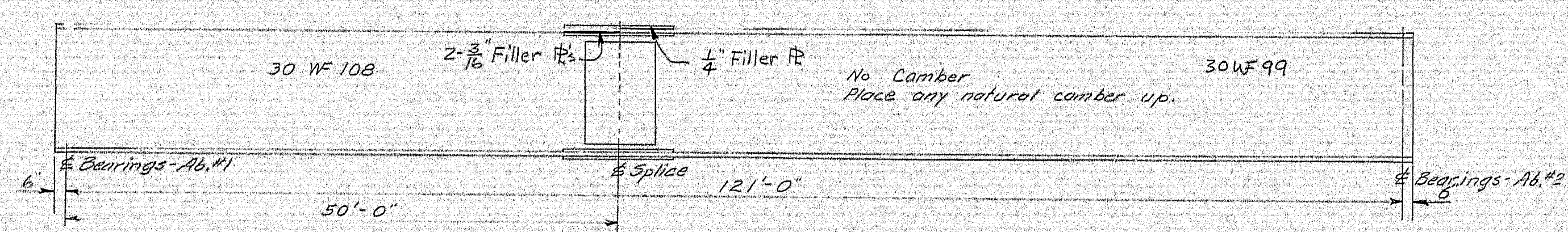


FRAMING PLAN - SOUTHBOUND (Framing Plan-Northbound is the same)

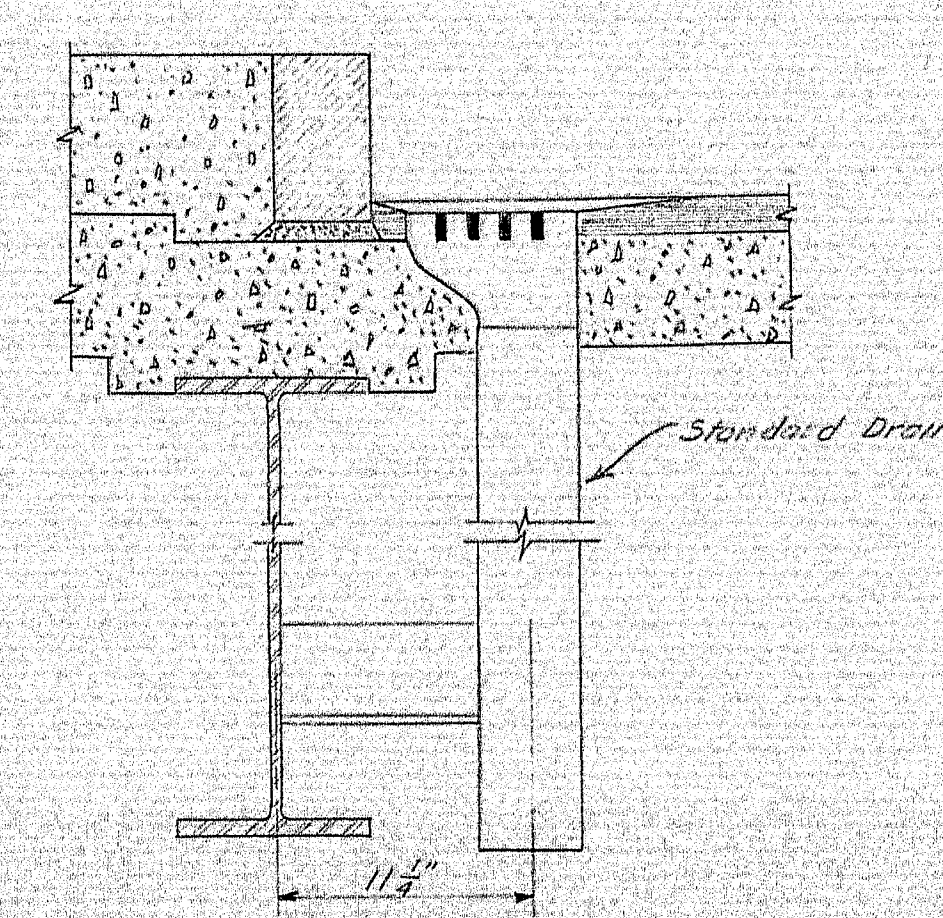
A - Type A Diaphragms
B - Type B Diaphragms
All dimensions are horizontal.



DEAD LOAD DEFLECTION CURVE
Excluding weight of structural steel.



STRINGER DETAIL
All dimensions are horizontal.



DRAIN DETAIL
1/2 Required

For locations see superstructure sheet (61)
For details see sheet BD 102-64.

	Bearing - Ab. 1	121'-0"	Splice	Bearing - Ab. 2
1	50'-0"			
2	-2.168%		-1.946%	
3	-2.192%		-1.943%	
4	-2.168%		-1.946%	
5	-2.168%		-1.932%	
6	-2.192%		-1.915%	
7	-2.168%		-1.918%	

SOUTHBOUND

	Bearing - Ab. 1	121'-0"	Splice	Bearing - Ab. 2
1	50'-0"			
2	-2.313%		-2.042%	
3	-2.313%		-2.042%	
4	-2.313%		-2.042%	
5	-2.313%		-2.028%	
6	-2.313%		-2.014%	
7	-2.313%		-2.014%	

NORTHBOUND

BEAM GRADES
(Along bottom of bottom flange)

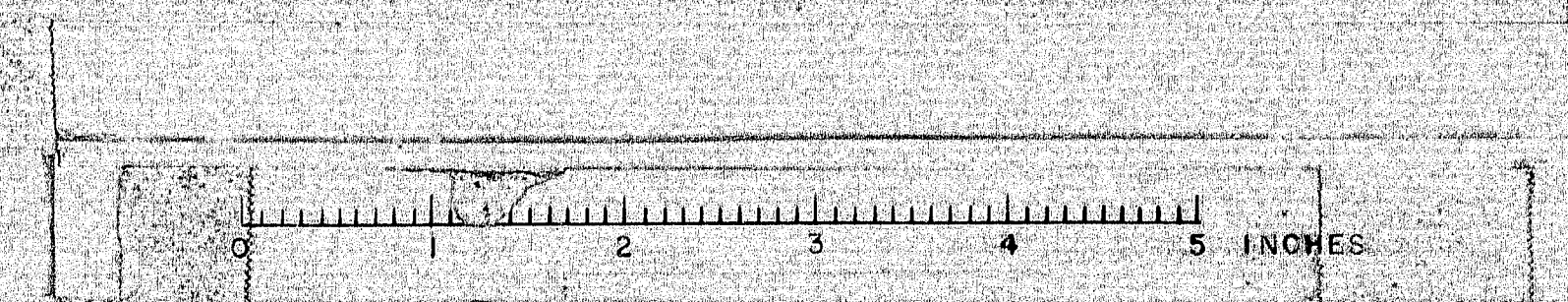
Footings:
EPB-1 38 Required
FB-1 12 Required

REFERENCES:
Armored Joints: Standard Details BD-104-64
Bearing Pedestals: Standard Details BD-101-64
Diaphragms: Standard Details BD-104-64
Drains: Standard Details BD-104-64
Splices: Standard Details BD-103-64

Note:
See General Plan for Structural Steel
Specifications and Classifications. (Sheet 57)

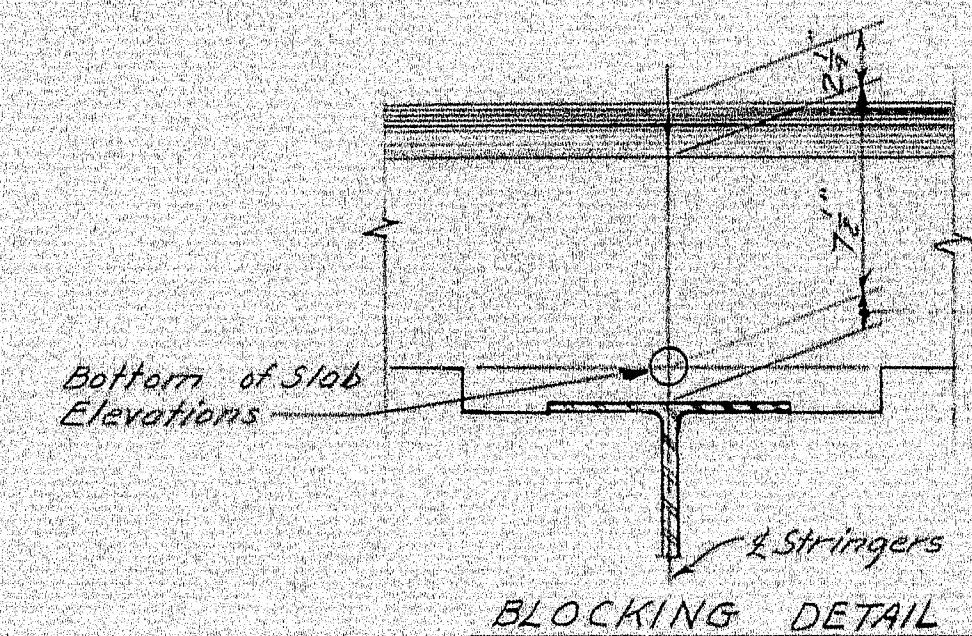
DESIGN - CDR TRACE - CDR CHECK - H/RQ	BRIDGE NO. SURVEY - PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95	
OVER	
ROUTE 116	
IN THE TOWN OF	
MEDWAY	
PENOBSCOT COUNTY	
STRUCTURAL STEEL	
SHEET 60 OF 93 AUGUSTA, MAINE OCT. 1964	

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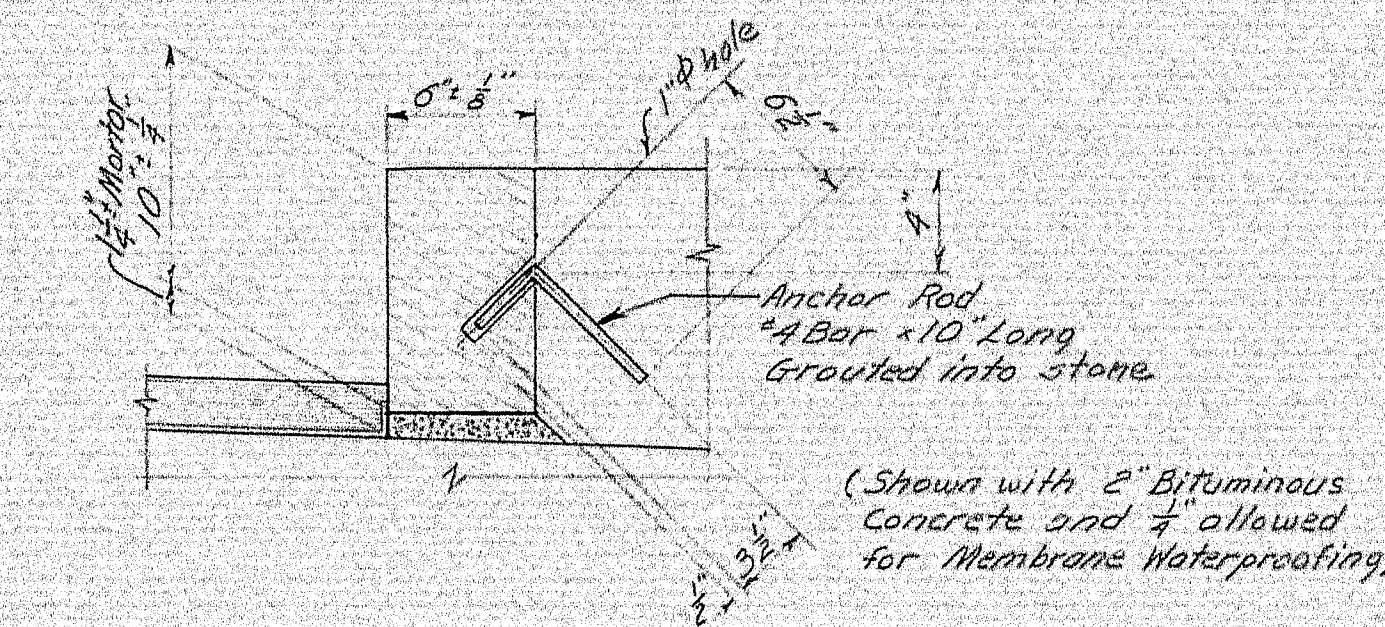
BOTTOM OF SLAB ELEVATIONS AT BLOCKING POINTS - NORTHBOUND																
Stringer Line	L Brg. Ab. #1	Span 1				L Brg. Pier #1	Span 2				L Brg. Pier #2	Span 3			L Brg. Ab. #2	
		10'-0"	20'-0"	21'-6"	31'-6"		10'-0"	20'-0"	22'-6"	32'-6"		10'-0"	17'-0"	27'-0"		
Line 1	289.86	289.64	289.41	289.37	289.14	288.90	288.68	288.47	288.41	288.20	287.99	287.79	287.66	287.45	287.25	Line 1
Line 2	289.97	289.75	289.52	289.48	289.24	289.01	288.79	288.58	288.52	288.31	288.10	287.90	287.77	287.57	287.37	Line 2
Line 3	290.08	289.86	289.63	289.59	289.35	289.12	288.90	288.69	288.63	288.42	288.21	288.01	287.88	287.68	287.48	Line 3
Line 4	289.98	289.76	289.53	289.49	289.26	289.02	288.80	288.59	288.54	288.32	288.12	287.92	287.79	287.59	287.39	Line 4
Line 5	289.82	289.60	289.37	289.33	289.10	288.86	288.64	288.43	288.38	288.16	287.95	287.77	287.63	287.43	287.24	Line 5
Line 6	289.66	289.44	289.21	289.17	288.94	288.70	288.48	288.27	288.22	288.00	287.80	287.61	287.47	287.28	287.08	Line 6

BOTTOM OF SLAB ELEVATIONS AT BLOCKING POINTS - SOUTHBOUND																
Stringer Line	# Brg. Ab. #1	Span 1				# Brg. Pier 1	Span 2				# Brg. Pier 2	Span 3			# Brg. Ab. #2	
		10'-0"	20'-0"	21'-6"	31'-6"		10'-0"	20'-0"	22'-6"	32'-6"		10'-0"	17'-0"	27'-0"		
Line 1	289.52	289.31	289.09	289.05	288.84	288.61	288.40	288.20	288.15	287.94	287.75	287.55	287.43	287.24	287.05	Line 1
Line 2	289.63	289.42	289.20	289.17	288.94	288.72	288.51	288.31	288.26	288.06	287.86	287.67	287.54	287.35	287.16	Line 2
Line 3	289.73	289.53	289.31	289.28	289.05	288.83	288.62	288.42	288.37	288.17	287.97	287.78	287.66	287.47	287.27	Line 3
Line 4	289.78	289.57	289.36	289.32	289.10	288.88	288.67	288.47	288.42	288.21	288.02	287.83	287.70	287.52	287.32	Line 4
Line 5	289.62	289.41	289.20	289.16	288.94	288.72	288.51	288.31	288.26	288.06	287.86	287.67	287.55	287.36	287.17	Line 5
Line 6	289.46	289.25	289.04	289.00	288.78	288.56	288.35	288.15	288.10	287.90	287.70	287.52	287.39	287.20	287.01	Line 6

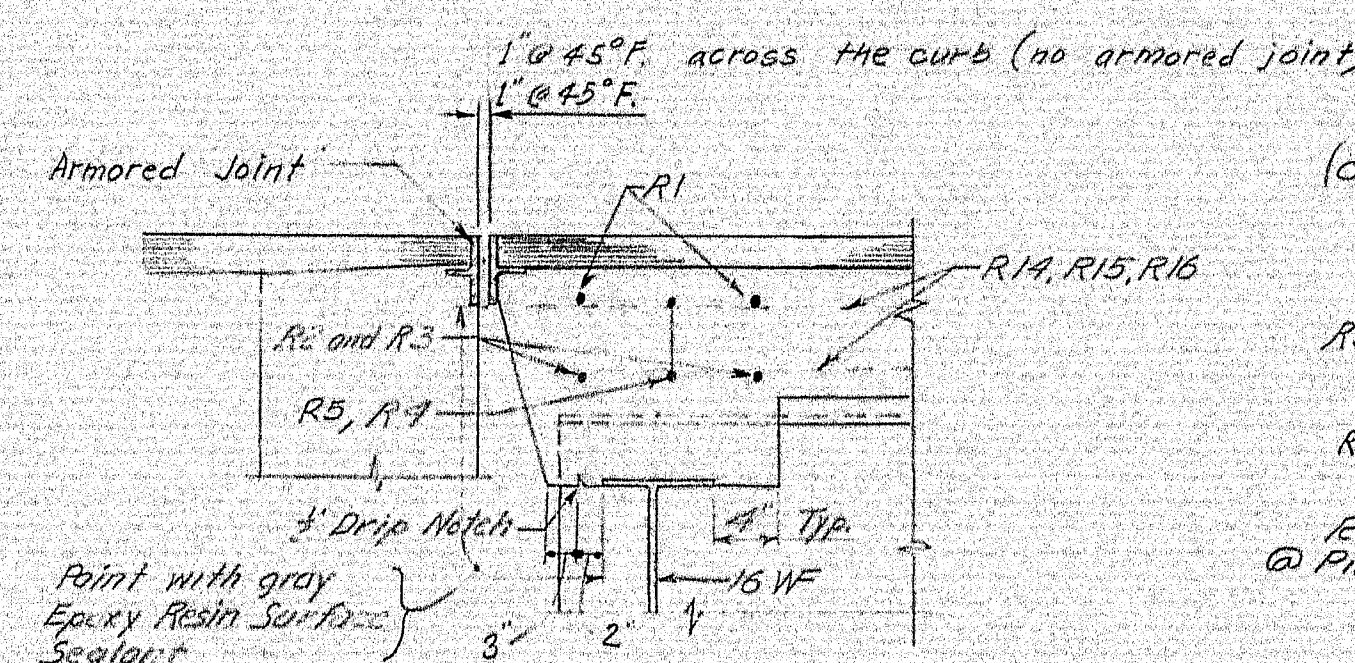


Blocking
& Bearing - Abut. #1 & Pier #1 = 1 1/2"
& Bearing - Pier #2 = 1 1/2"
& Bearing - Abut. #2 = 1 1/2"
(DO NOT use for setting forms)

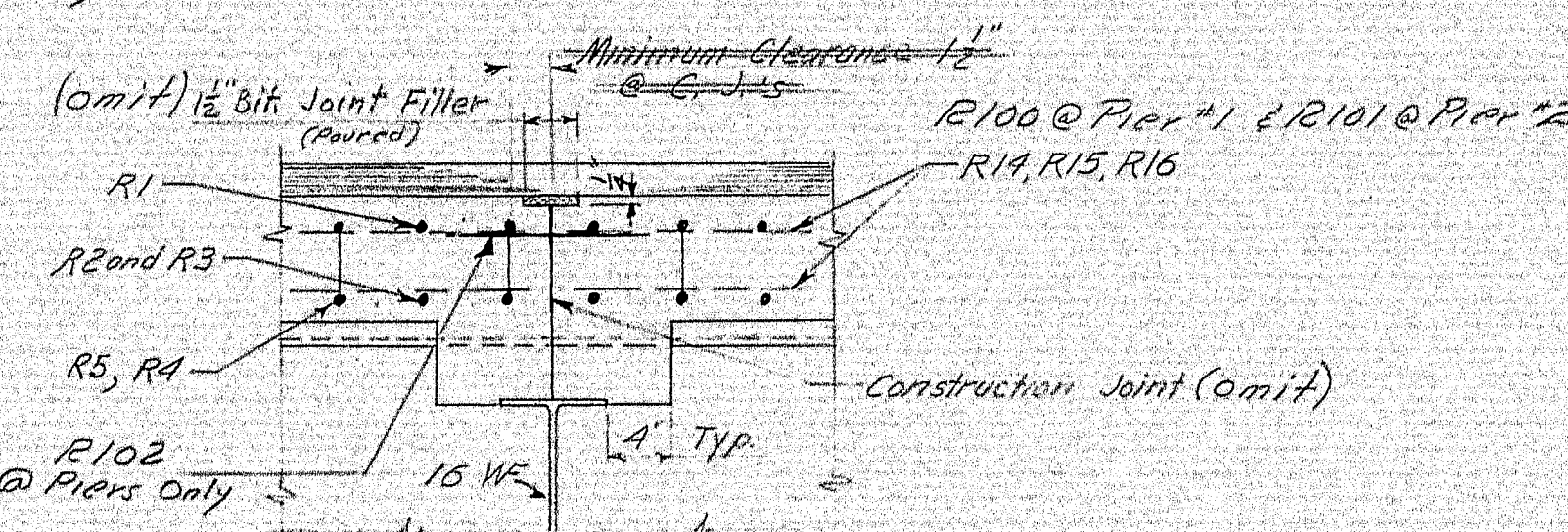
Note:
To compensate for dead load deflections as well as possible irregularities in beams, set the bottom of slab elevations at the points indicated before any of the slab formwork is started.



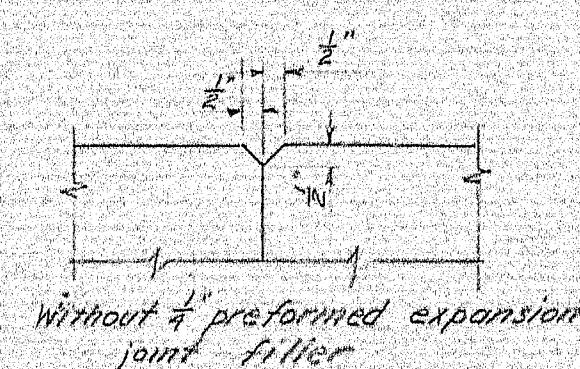
TYPICAL SECTION-VERTICAL BRIDGE CURB - TYPE I



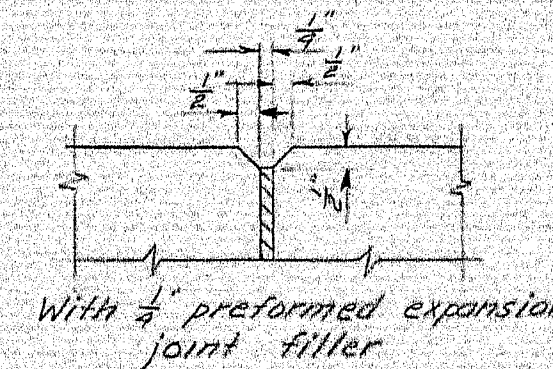
SECTION A-A
Typical @ Abutments



SECTION B-B
Typical @ Piers
Revised 8-5-64 H.L.D.



Without preformed expansion joint filler



With preformed expansion joint filler

1\"/>

GENERAL SUPERSTRUCTURE NOTES

- At joints in curbs and Vertical Bridge Curbs over Piers use 4\"/>

Revised Aug. 5, 1965 To place roadway slab in one operation.

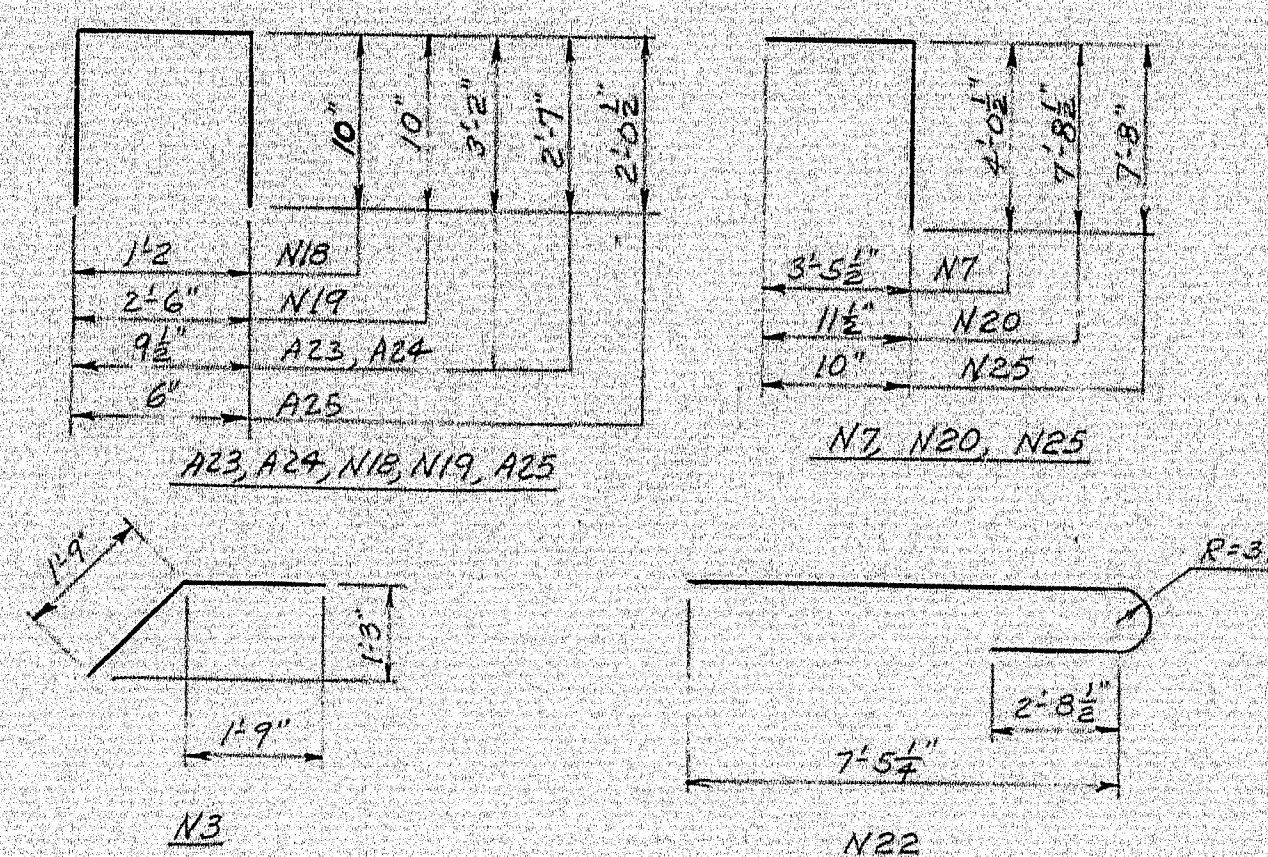
Note:
Work this sheet with sheet 61.

DESIGN - CCH	BRIDGE NO.
TRACE - CCH	SURVEY
CHECK - HPA	PLOT
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95	
OVER	
ROUTE 116	
IN THE TOWN OF	
MEDWAY	
PENOBSCOT COUNTY	
SUPERSTRUCTURE DETAILS & BLOCKING	
SHEET 62 OF 93 AUGUSTA, MAINE OCT. 1964	

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B. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95- 8 (62)	63	93

ABUTMENTS



MARK	SIZE	NO.	LENGTH	LOCATION
N13	#5	16	20'-10"	Bridge seat
N14	#5	16	23'-0"	Bridge seat
N15	#5	16	23'-0"	Bridge seat
N16	#4	24	20'-10"	Backwall
N17	#4	24	23'-0"	Backwall
N21	#4	64	5'-9"	Wings and curbs
N23	#4	8	4'-9"	Curbs
N24	#5	8	6'-6"	Bridge seat to curtain wall
N26	#5	40	2'-0"	Wings to curb
N27	#5	40	8'-8"	Wing to curb

BENT BARS

MARK	SIZE	NO.	LENGTH	LOCATION
A25	#5	8	4'-7"	End Post
N3	#6	104	3'-6"	Backwall to approach
N7	#5	112	7'-6"	Bridge seat
N18	#4	48	2'-10"	Bearing pads
N19	#4	48	4'-2"	Bearing pads
N20	#4	24	8'-8"	Wings
N22	#4	16	11'-0"	Wings and curtain walls
N25	#5	24	8'-6"	Wings
A23	#5	16	7'-2"	End Post
A24	#5	8	6'-0"	End Post

STRAIGHT BARS

N1	#6	60D	14'-6"	Approach Slab
N2	#4	80	37'-11"	Approach Slab
N4	#5	136	2'-9"	Footing to bridge seat
N5	#5	112	7'-7"	Footing to backwall
N6	#5	112	2'-9"	Bridge seat to backwall
N8	#5	224	3'-5"	Backwall
N9	#6	352	5'-0"	Footing
N10	#6	24	16'-7"	Footing
N11	#6	24	28'-11"	Footing
N12	#5	16	20'-10"	Bridge seat

R100	"5	66	12'-9"	Pier #1	Top of slab
R101	"5	66	11'-11"	Pier #2	Top of slab
R102	"5	136	2'-10"	Slab Pier #1 & Pier #2	splice bar

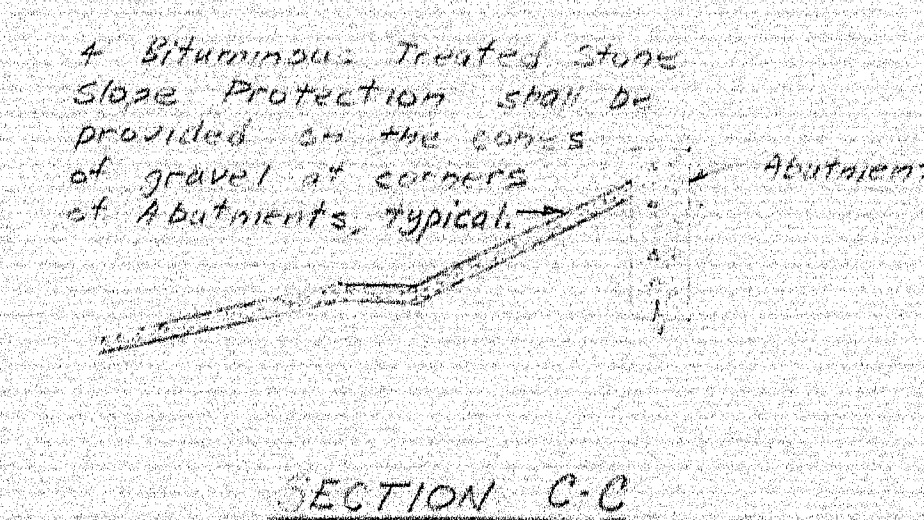
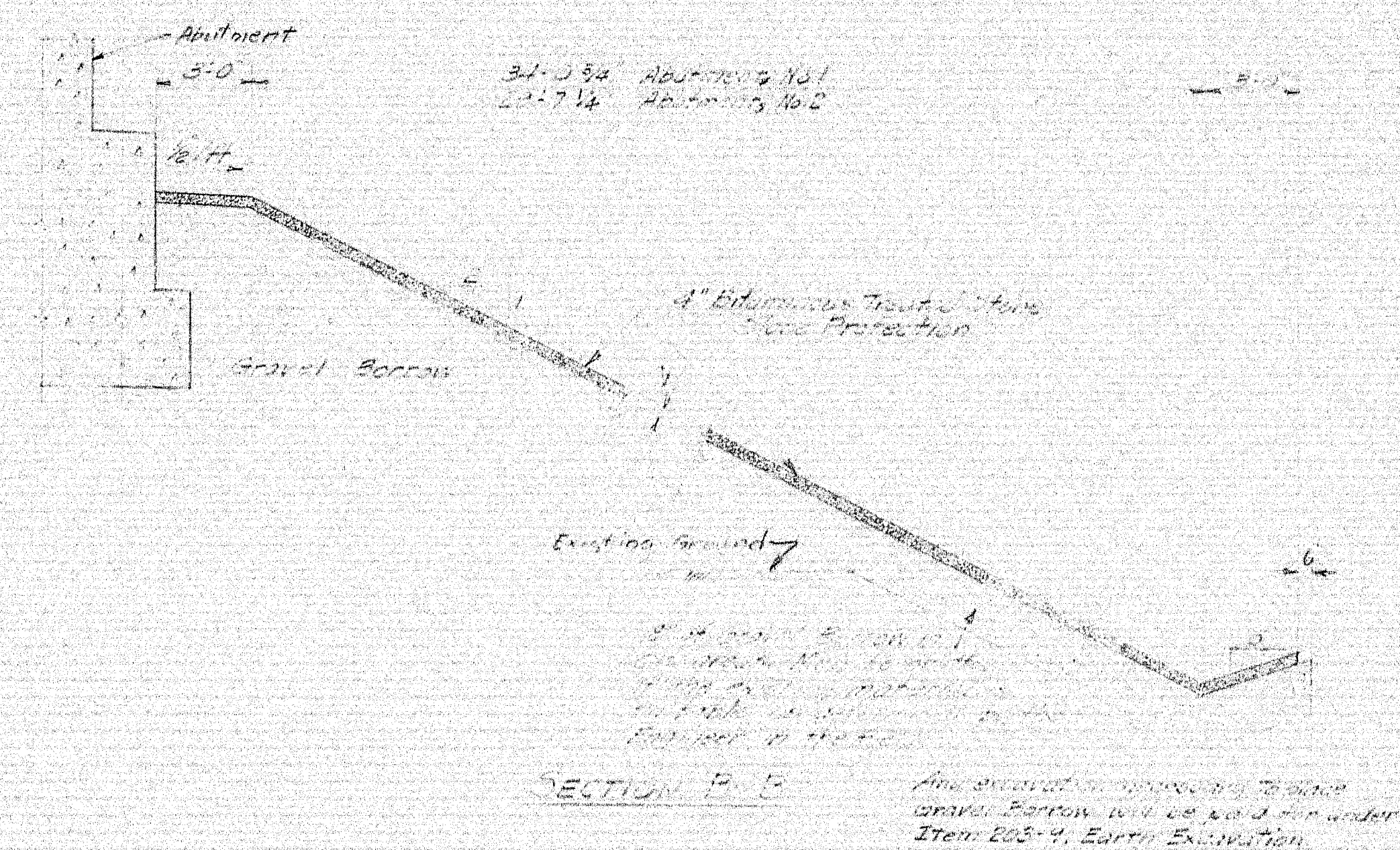
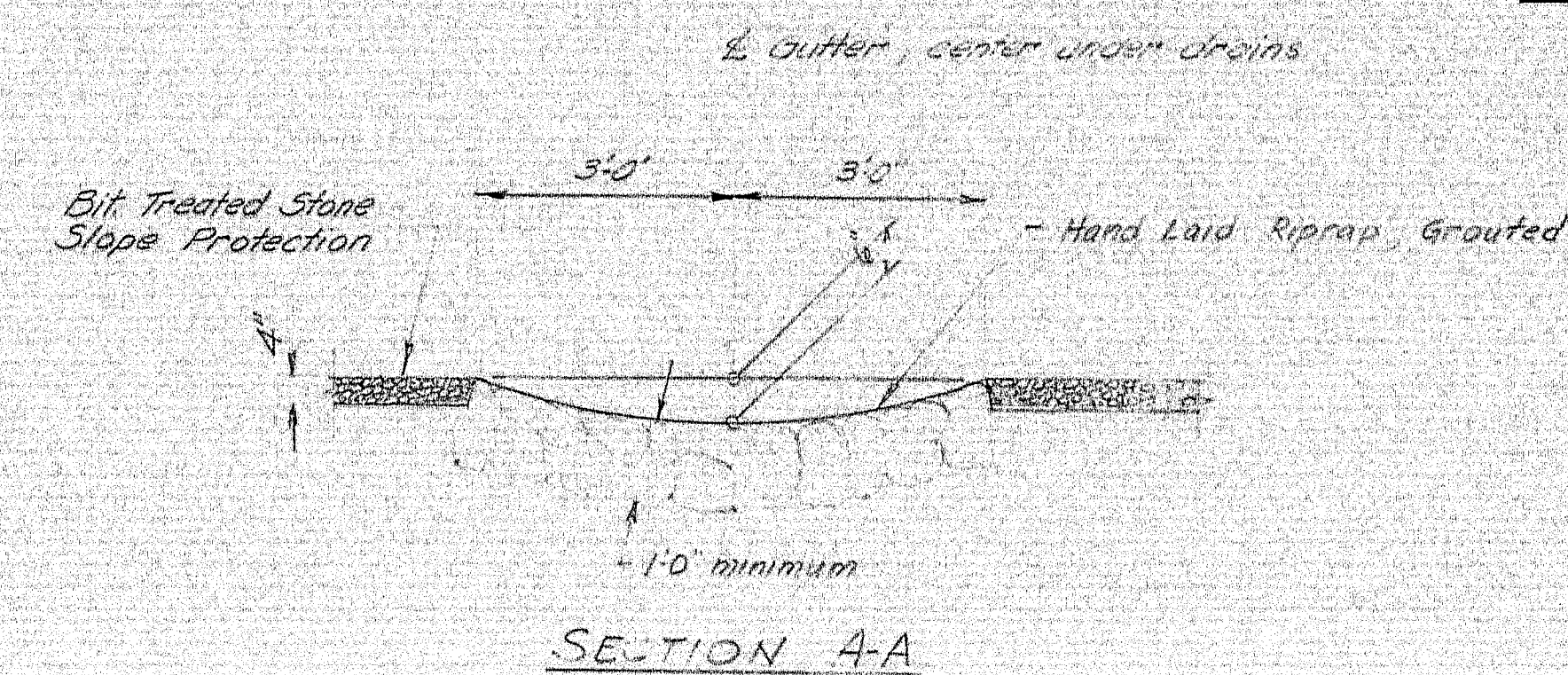
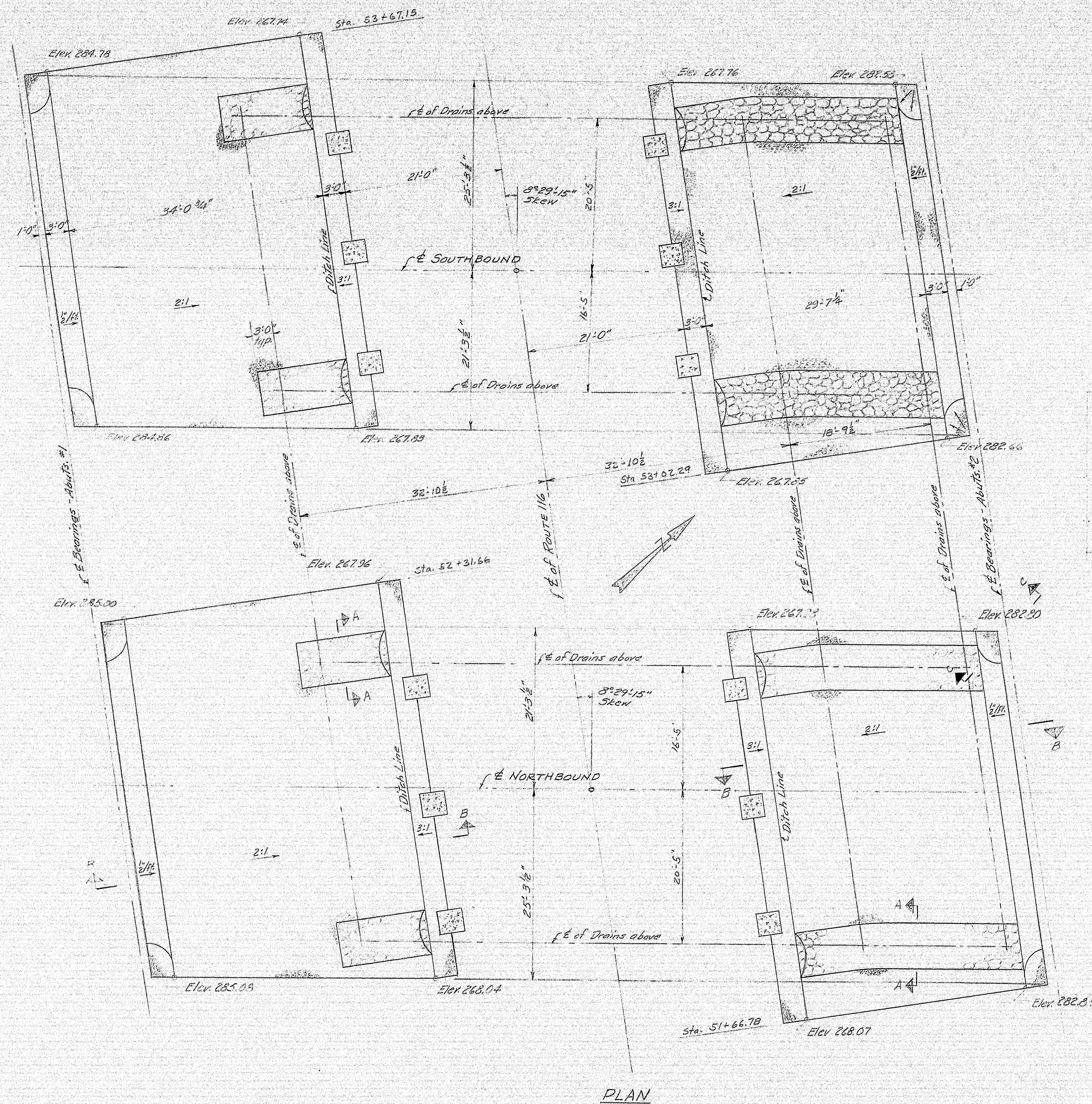
1. All dimensions are to centerline of bars
2. All bars shall be Intermediate Grade steel.

DESIGN- <i>CDH</i>	BRIDGE NO.
TRACE- <i>CDH</i>	SURVEY-
CHECK- <i>HAG</i>	PLOT-

STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95
OVER
ROUTE 116
IN THE TOWN OF
MEDWAY
PENOBSCOT COUNTY
REINFORCING STEEL SCHEDULE
SHEET 63 OF 93 AUGUSTA, MAINE OCT.

99-68

B. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEET
1	MAINE	1-95-8 (62)	64	93



DESIGN - CDH
TRACE - DET. F.H. Barnes
CHECK - HRQ

BRIDGE NO.
SURVEY
PLOT -

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95

OVER

ROUTE 116

IN THE TOWN OF

MEDWAY

PENOBSCOT COUNTY

SLOPE PROTECTION

SHEET 64 OF 93 AUGUSTA, MAINE OCT. 1964

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95

OVER

ROUTE 116

IN THE TOWN OF

MEDWAY

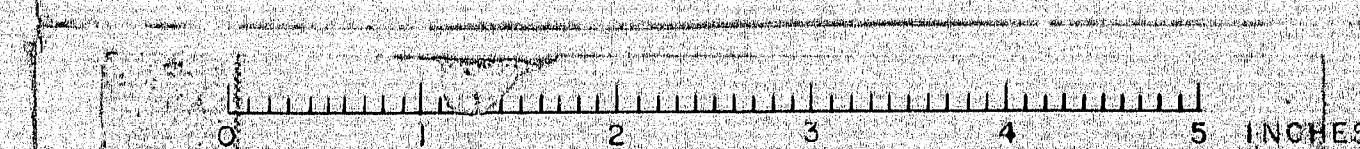
PENOBSCOT COUNTY

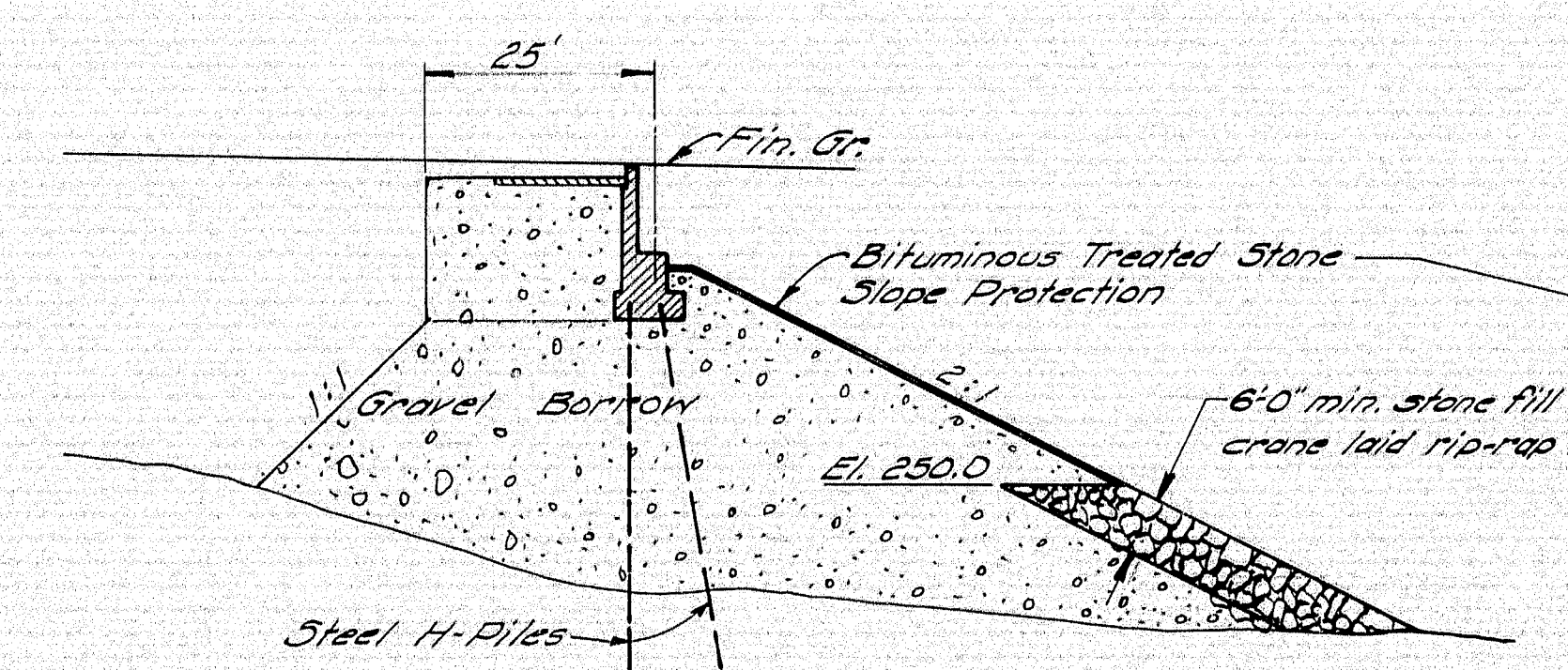
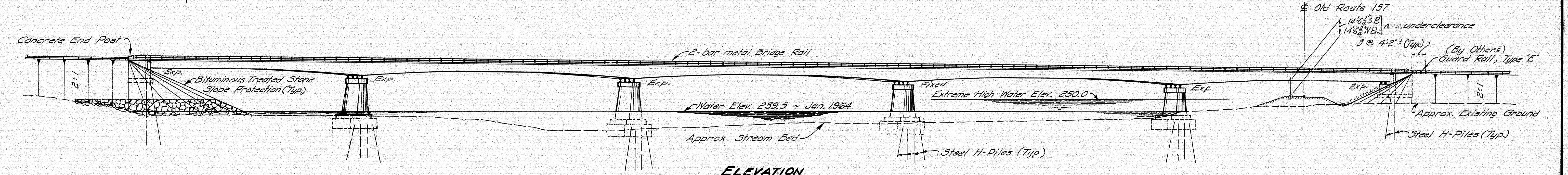
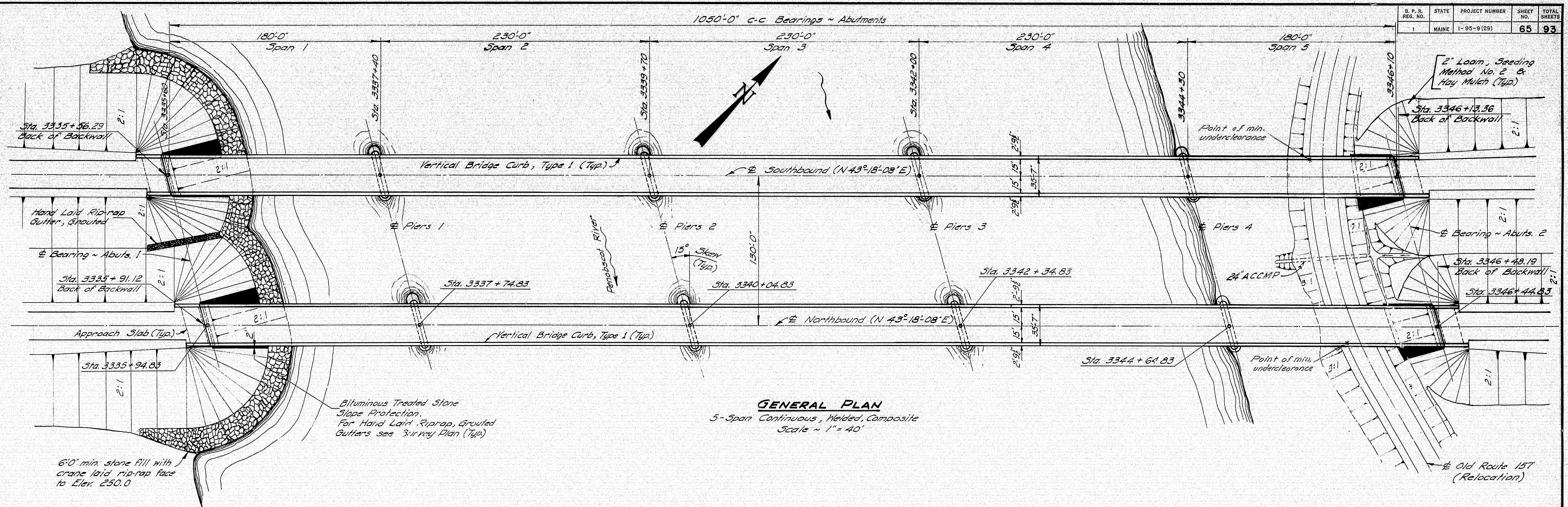
SLOPE PROTECTION

SHEET 64 OF 93 AUGUSTA, MAINE

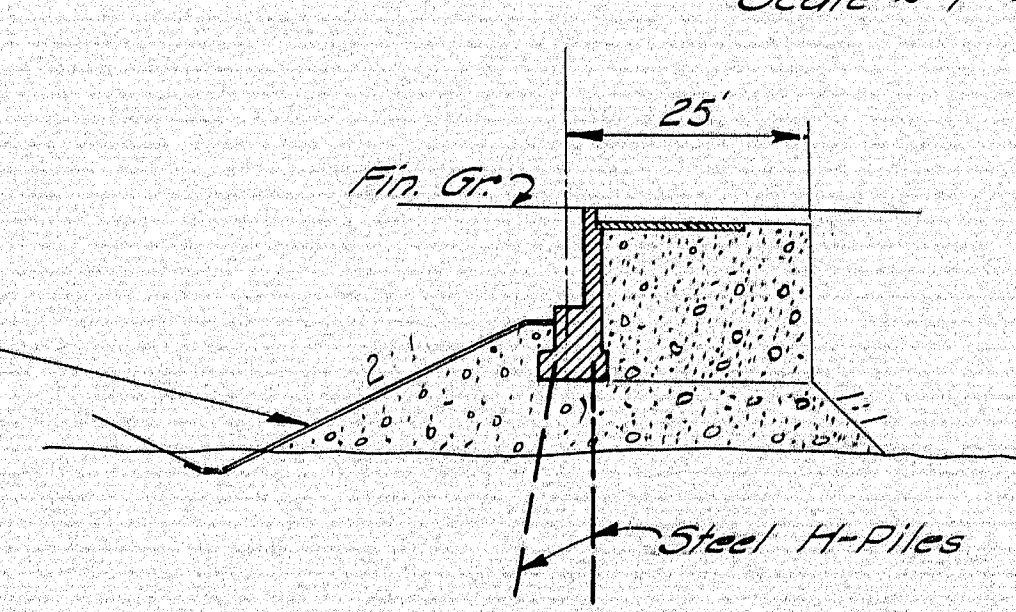
OCT. 1964

99-69





TYPICAL SECTION - ABUTS. 1



TYPICAL SECTION - ABUTS. 2

SPECIFICATIONS
 DESIGN ~ A.A.S.H.O. Standard Specifications for Highway Bridges 1961, and Interim Revisions, 1961, 62, 63, 64
 CONTRACT ~ State of Maine, State Highway Commission, Standard Specifications for Highways and Bridges, Revision of January 1956, and Supplemental Specifications, February 1960.
 LOADING ~ HS 20-44, as modified for Interstate.

STRUCTURAL STEEL - ALLOWABLE STRESSES & ASTM CLASSIFICATION
 A36 ~ 20,000 psi
 A441 ~ 25,000 psi (for $\frac{3}{4}$ " to $1\frac{1}{2}$ " R incl.)
 High Tensile Strength Bolts ~ A325
 Haunched section of girders over piers (Web and flange plates only) ~ A441
 All other girder material, stiffeners, bracing, splices and incidentals ~ A36
 Pedestals:
 Pins ~ A235 (Class E) or A108 (Grade 102 to 1030)
 Anchor Bolts ~ A36 or A307
 All other ~ A36

CONCRETE CLASSIFICATIONS
 Piers: _____
 Seals ~ Class 'C'
 Remainder ~ Class 'B'
 All other: ~ Class 'A'
ALLOWABLE STRESSES
 Concrete ~ $R_c = 1200$ psi, $n = 10$
 Reinforcing Steel ~ $R_s = 20,000$ psi

DESIGN - M.C.E.
 TRACE - G.M.C.
 CHECK - J.H.R.
 BRIDGE NO. _____
 SURVEY - PLOT
 STATE HIGHWAY COMMISSION
 BRIDGE DIVISION
INTERSTATE 95
 OVER
PENOBSCOT RIVER
 IN THE TOWN OF
MEDWAY
PENOBSCOT COUNTY
 GENERAL PLAN & ELEVATION
 SHEET 65 OF 93 AUGUSTA, MAINE OCTOBER, 1964

99-70

